

ZONING CHANGE REVIEW SHEET

CASE: C14-2024-0170 (Duval PDA)

DISTRICT: 6

ADDRESS: 5501 Duval Road; 11705, 11755, 11755 ½, 11757 ½, 11781 ½, 11785 ½, 11801 ½, Research Boulevard SVRD NB, 11900, 11900 ½, 11902 ½, 11904 ½ West Cow Path

ZONING FROM: LI, LI-CO, LO

TO: LI-PDA

In this case, the applicant is requesting to add a PDA, Planned Development District, overlay to the existing LI zoning to redevelop the property with a mixture of uses (*Please see Applicant's Request Letter - Exhibit C*).

1) Through the PDA overlay the following uses are additional permitted uses to the LI base district:

Permitted Uses by Right:

Residential Uses

Multi-Family Residential
Townhouse Residential
Condominium Residential

Commercial Uses

Performance Venue
Cocktail Lounge
Outdoor Entertainment
Research Assembly Services
Research Testing Services
Research Warehousing Services

Civic Uses

Club or Lodge

2) The following use shall be a prohibited uses on the property:

Prohibited Uses:

Automotive Repair Services
Building Maintenance Services
Campground Commercial
Off-Street Parking
Convenience Storage
Drop-Off Recycling Collection Facility
Funeral Services
Monument Retail Sales
Pedicab Storage and Dispatch
Scrap and Salvage
Vehicle Storage

Recycling Center
Resource Extraction
Railroad Facilities

3) In addition, the applicant proposes the following modification to the LI base zoning district through the PDA:

Site Development Regulations:

Minimum Lot Size: 5,750 sq ft

Minimum Lot Width: 50 ft

Maximum Height: 60 ft

Minimum Setbacks:

A 50-ft minimum setback shall be required from property zoned SF-5 or more restrictive.

Front Yard: 0 ft:

Street Side Yard: 0 ft

Interior Side Yard: 0 ft

Rear Yard: 0 ft

Maximum Floor Area Ratio (FAR): 2:1

Maximum Impervious Cover: Set forth by Watershed

Maximum Building Coverage: Set forth by Watershed

SITE AREA: 57.21 acres

PROPERTY OWNER: Karlin Duval, LLC

APPLICANT/AGENT: Armbrust & Brown PLLC (Richard T. Suttle, Jr.)

CASE MANAGER: Sherri Sirwaitis (512-974-3057, sherri.sirwaitis@austintexas.gov)

STAFF RECOMMEDATION:

Staff recommends LI-PDA, Limited Industrial-Planned Development Area Combining District zoning, to change a condition of zoning. The PDA will be subject to the following additional conditions:

1) Cocktail Lounge use will be limited to 30,000 sq. ft. on the property.

2) Outdoor Entertainment use will be limited to 20,000 sq. ft. on the property.

3) Buffer Zone:

A 75-foot wide building setback shall be established and maintained between property developed with the Residential uses listed below:

Multi-Family Residential

Townhouse Residential

Condominium Residential

And the following Commercial and Industrial uses listed below:

**Research Assembly Services
Research Testing Services
Research Warehousing Services
Construction Sales and Services
Automotive Rentals
Automotive Sales
Custom Manufacturing
General Warehousing and Distribution
Light Manufacturing
Limited Warehousing and Distribution**

4) Prohibit the following additional uses on the property:

Basic Industry

5) The development of the property will be subject to the updated recommendations in the US 183 and Duval Office Complex TIA Compliance Memorandum from the Transportation and Public Works Department (TPW) dated February 8, 2022 – Exhibit D.

ZONING AND PLATTING COMMISSION ACTION / RECOMMENDATION:

April 15, 2025: Approved staff's request for a postponement to June 3, 2025 by consent (9-0, S. Boone and T. Major-absent); R. Puzycki-1st, A. Flores-2nd.

June 3, 2025: Approved staff's recommendation of LI-PDA zoning by consent (9-0, B. Greenberg-No, C. Tschoepe-arrived late, F. De Portu-absent); L. Stern-1st, A. Flores-2nd.

CITY COUNCIL ACTION:

July 24, 2025

ORDINANCE NUMBER:

ISSUES: N/A

CASE MANAGER COMMENTS:

The property in question is the former 3M Company campus that is zoned LI, LI-CO and LO. It is currently a 57.21 acre vacant tract of land with access to Research Boulevard, Duval Road and West Cow Path. The parcel directly to the north is zoned LI and is undeveloped. Further to the north there is a tract zoned IP that contains an office development (Riata Corporate Park) that fronts Riata Trace Parkway. To the south, there is right-of-way zoned LO and a retail center (Angus Square) zoned GR. There are single-family residential lots zoned SF-2, SF-3, SF-1 and RR to the east and west. Near the northeast intersection of Duval Road and West Cow Path there are detention ponds and a primary school (Davis Elementary School) zoned SF-2 and a retirement home (Brookdale North Austin) zoned LO-CO. Along Research Boulevard to the west, there is a vacant office use, zoned GR with a pad site of CS zoning that contains a pawn shop services use (Cash America Pawn). In this rezoning request, the applicant is asking to add a PDA, Planned Development District, overlay to the existing LI zoning to redevelop the property with a mixture of uses including 1,200 multifamily residential units and 30,000 square feet of retail and restaurant uses (*please see Applicant's Summary Letter – Exhibit C*).

The staff recommends Limited Industrial-Planned Development Area District zoning as the property meets the intent of the LI-PDA combining district. The zoning would be compatible and consistent with the surrounding uses because there are office, commercial and industrial uses located to the north and south and residential and civic uses to the east and west. The staff recommendation includes proposed compatibility setbacks between the residential uses and more intensive commercial/industrial uses on the property. This has been done in other PDA cases throughout the city to provide a buffer between these types of uses. In addition, the staff recommends prohibiting Basic Industry as this use is incompatible with residential development:

§ 25-2-5 - INDUSTRIAL USES DESCRIBED.

- 1) BASIC INDUSTRY use is the use of a site for:
 - (a) the basic processing and manufacturing of materials or products predominately from extracted or raw materials;
 - (b) storage or manufacturing processes that involve flammable or explosive materials; or
 - (c) storage or manufacturing processes that involve hazardous or commonly recognized offensive conditions, including poultry processing.

The proposed LI-PDA zoning would allow this site to be redeveloped with a mixture of higher density residential, office, commercial and industrial uses. This location is appropriate for the proposed mixture of uses because the property in question is at the northeast intersection of a major arterial roadway, Research Boulevard/U.S. Highway 183, a collector roadway, Duval Road, and a local roadway, West Cow Path. In addition, the property is located 0.25 miles from Jollyville Road Activity Corridor and is 0.10 miles from a Capital Metro bus stop along Research Boulevard Service Road North Bound. There is a sidewalk

and bike lane present along Duval Road and a sidewalk along West Cow Path, with civic services such as a primary school and neighborhood park nearby.

BASIS OF RECOMMENDATION:

1. *The proposed zoning should be consistent with the purpose statement of the district sought.*

Limited Industrial Service district is intended as an area primarily for commercial services and limited manufacturing uses, generally on moderately sized sites.

Planned Development Area combining district is intended for combination with selected commercial and industrial base districts, in order to modify base district provisions as necessary to allow for appropriate industrial and commercial uses or to reflect the terms of a Planned Development Area agreement following annexation of properties subject to such an agreement.

2. *The proposed zoning should promote consistency and orderly planning.*

The LI-PDA zoning district would be compatible and consistent with the surrounding uses because there are commercial and industrial uses located to the north, south and west and residential uses to the east and west. The PDA request includes a 50-ft minimum setback from property zoned SF-5 or more restrictive and the staff's recommendation adds a setback/buffer zone between the residential and more intensive commercial/industrial uses on the property.

3. *Zoning changes should promote an orderly relationship among land uses.*

LI-PDA zoning would allow for this site to be developed with a mixture of residential, office, commercial and industrial uses. This location is appropriate for the proposed mixture of uses because the property in question is 0.25 miles from Jollyville Road Activity Corridor and in an area with civic amenities such as schools and parks.

4. *The proposed zoning should allow for a reasonable use of the property.*

The LI-PDA zoning district would allow for a fair and reasonable use of the site. The proposed addition of the Planned Development Area Combining District will allow for higher density residential uses, which will provide desirable housing opportunities for the people that work within this development and in the surrounding commercial and industrial areas. There are public transportation options available nearby as the property is adjacent to bus stop along Research Boulevard Service Road North Bound and there is a bike lane present along Duval Road.

EXISTING ZONING AND LAND USES:

	ZONING	LAND USES
<i>Site</i>	LI, LI-CO and LO	Vacant – Former 3M Company Campus
<i>North</i>	LI, IP	Undeveloped Area, Office (Riata Corporate Park)
<i>East</i>	SF-1, RR, SF-2, LO-CO	Single-Family Residences, Detention, Public Primary School (Davis Elementary School), Retirement Home (Brookdale North Austin)
<i>South</i>	LO, GR	ROW, Restaurant (Short Stop) Retail Center (Angus Square)
<i>West</i>	SF-2, SF-3, GR, CS	Single-Family Residences, Vacant Office Building, Pawn Shop (Cash America Pawn)

NEIGHBORHOOD PLANNING AREA: N/A

WATERSHED: Walnut Creek

CAPITOL VIEW CORRIDOR: N/A

SCENIC ROADWAY:

NEIGHBORHOOD ORGANIZATIONS:

Angus Valley Area Neighborhood Association,
 Austin Independent School District
 Bull Creek Foundation
 Friends of Angus Valley
 Friends of Austin Neighborhoods
 Homeless Neighborhood Association
 North Oaks Neighborhood Association
 Long Canyon Homeowners Association
 Mountain Neighborhood Association (MNA)
 Raintree Estates
 Save Our Springs Alliance
 Summit Oaks Neighborhood Association
 TNR BCP-Travis County Natural Resources

SCHOOLS: Austin I.S.D, Round Rock I.S.D.

AREA CASE HISTORIES:

NUMBER	REQUEST	COMMISSION	CITY COUNCIL
C14-2020-0146 (11705 Research Blvd Zoning)	LI-CO to LI-CO	7/06/21: Motion to deny the rezoning request (5-2-1, Timothy Bray and Ellen Ray - No, Carrie Thompson-abstain); Betsy Greenberg-1st, David King-2nd. Commissioners	11/14/21: Approved LI-CO zoning, with conditions, on 1st reading by consent (11-0); P. Renteria-1st, P. Ellis-2nd.

		<p>Cesar Acosta, Nadia Barrera-Ramirez and Hank Smith were absent. Motion failed.</p> <p>Forward to City Council without a recommendation.</p>	
<p>C14-2017-0085 (Charles Ford Rezoning: 12101 ½ Conrad Road)</p>	<p>SF-2 to SF-3</p>	<p>9/05/17: Motion to approve staff's recommendation of SF-3 zoning made by S. Lavani-1st, B. Evans-2nd.</p> <p>Substitute motion to approve SF-3 zoning, with a conditional overlay to prohibit Duplex Residential use and to provide on-site parking for accessory dwelling units made by A. Denkler, B. Evans-2nd. Vote: (5-6, D. King, A. Aguirre, J. Duncan, J. Kiolbassa, B. Greenberg, Y. Flores-No). The motion failed.</p> <p>Returned to the original motion to approve staff's recommendation of SF-3 zoning (4-7, D. King, A. Denkler, A. Aguirre, J. Duncan, J. Kiolbassa, B. Greenberg, Y. Flores-No). The motion failed.</p> <p>Case sent forward to the City Council without a recommendation.</p>	<p>10/12/17: Approved the staff's recommendation for SF-3 zoning on 1st reading (10-1, L. Pool-No); J. Flanningan-1st, D. Garza-2nd.</p> <p>11/09/17: Ordinance No. 20171109-063 for SF-3 district zoning was approved on Council Member Alter's motion, Council Member Houston's second on a 10-1 vote. Council Member Pool voted nay.</p>
<p>C14-2007-0228 11722 Bell Avenue)</p>	<p>SF-2 to SF-3</p>	<p>12/18/07: Approved staff recommendation of SF-3 zoning by consent (8-0); K. Jackson-1st, J. Shieh-2nd.</p>	<p>1/17/08 : Approved SF-3 zoning by consent (7-0); all 3 readings</p>
<p>C14-00-2008 (Summit Oaks Office Park: Howlett Ct.)</p>	<p>SF-2 to LO</p>	<p>2/29/00: Approved staff recommendation of LO zoning, with prohibition on Medical Office uses and a 2,000 vehicle trip per day limit (7-0)</p>	<p>3/30/00: Approved LO-CO zoning, with conditions as recommended by Planning Commission on all 3 readings (6-0)</p>
<p>C14-98-0155 (Tweed Court: 12000 Block of Bell Avenue and Tweed Court)</p>	<p>LO, GR to LO-CO</p>	<p>11/10/98: Approved staff rec. of LO-CO, with conditions for no vehicular access to Bell Avenue, access to Tweed Court is permitted only if TxDoT prohibits access to U.S. 183 and a 2,000 vehicle trip per day limit (7-0)</p>	<p>1/14/99: Approved PC rec. of LO-CO zoning, with conditions (7-0); 1st reading</p> <p>3/25/99: Approved LO-CO zoning with conditions (6-0); 2nd/3rd readings</p>
<p>C14-89-0009 (Wilson Tract: 11805-11841 Block of North</p>	<p>LO to LI</p>	<p>6/27/89: Approved LI-CO zoning with conditions: 1) 40 foot height limit, 2) prohibit Scrap and Salvage Services, Convenience Storage, Automotive Rentals, Automotive</p>	<p>7/27/89: Approved LI zoning, with conditions, on 1st reading</p> <p>1/18/90: Approved LI-CO zoning, with conditions, on 2nd/3rd readings</p>

U.S. Highway 183)		Repair Services, Automotive Sales, Automotive Washing, Resource Extraction, Hotel-motel, Basic Industry and Vehicle Storage uses, 3) prohibit the following uses on the rear portion of the property for a depth of 150 feet: General Warehousing and Distribution, Equipment Sales, Equipment Repair Services and Kennels, and 4) the property is subject to TIA conditions.	
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RELATED CASES:

C14-85-244; C14-89-0009; C14-2020-0146 - Previous Zoning Cases
 CS-67-64, C8s-77-244; C8s-78-246; C8-2012-0017.0A – Subdivision Cases
 SP-2021-0108D; SP-2021-0109C; SP-97-0166CF – Site Plan Cases

OTHER STAFF COMMENTS:

Comprehensive Planning

Project Name and Proposed Use: 5501 DUVAL ROAD. C14-2024-0170. Project: Duval PDA. 57.21 acres from LI, LI-CO.LO to LI-PDA. Existing: vacant. Proposed: 1,200 multifamily residential units and 30,000 square feet of retail and restaurant.

Yes	Imagine Austin Decision Guidelines
Complete Community Measures *	
Y	Imagine Austin Growth Concept Map: Located within or adjacent to an Imagine Austin Activity Center, Imagine Austin Activity Corridor, or Imagine Austin Job Center as identified the Growth Concept Map. Names of Activity Centers/Activity Corridors/Job Centers *: <ul style="list-style-type: none"> • 0.25 miles from Jollyville Road Activity Corridor
Y	Mobility and Public Transit *: Located within 0.25 miles of public transit stop and/or light rail station. <ul style="list-style-type: none"> • 0.10 miles from bus stop along Research Blvd SvrD NB
Y	Mobility and Bike/Ped Access *: Adjoins a public sidewalk, shared path, and/or bike lane. <ul style="list-style-type: none"> • Sidewalk and bike lane present along Duval Rd; sidewalk present along W Cow Path
Y	Connectivity, Good and Services, Employment *: Provides or is located within 0.50 miles to goods and services, and/or employment center. <ul style="list-style-type: none"> • Goods and Services available along Research Blvd and Duval Rd
	Connectivity and Food Access *: Provides or is located within 0.50 miles of a grocery store/farmers market.
Y	Connectivity and Education *: Located within 0.50 miles from a public school or university. <ul style="list-style-type: none"> • 0.3 miles to Davis Elementary School
Y	Connectivity and Healthy Living *: Provides or is located within 0.50 miles from a recreation area, park or walking trail. <ul style="list-style-type: none"> • 0.3 miles to Davis Elementary School park
Y	Connectivity and Health *: Provides or is located within 0.50 miles of health facility (ex: hospital, urgent care, doctor’s office, drugstore clinic, and/or specialized outpatient care.)

	<ul style="list-style-type: none"> • Multiple health facility options located at the intersection of Duval Rd and Jollyville Rd within 0.5 miles of the site
Y	Housing Choice *: Expands the number of units and housing choice that suits a variety of household sizes, incomes, and lifestyle needs of a diverse population (ex: apartments, triplex, granny flat, live/work units, cottage homes, and townhomes) in support of Imagine Austin and the Strategic Housing Blueprint.
	Housing Affordability *: Provides a minimum of 10% of units for workforce housing (80% MFI or less) and/or fee in lieu for affordable housing.
Y	Mixed use *: Provides a mix of residential and non-industrial uses.
	Culture and Creative Economy *: Provides or is located within 0.50 miles of a cultural resource (ex: library, theater, museum, cultural center).
	Culture and Historic Preservation : Preserves or enhances a historically and/or culturally significant site.
	Creative Economy : Expands Austin’s creative economy (ex: live music venue, art studio, film, digital, theater.)
	Workforce Development, the Economy and Education : Expands the economic base by creating permanent jobs, especially in industries that are currently not represented in a particular area or that promotes a new technology, and/or promotes educational opportunities and workforce development training.
	Industrial Land : Preserves or enhances industrial land.
	Not located over Edwards Aquifer Contributing Zone or Edwards Aquifer Recharge Zone
9	Number of “Yes’s”

Drainage

The developer is required to submit a pre- and post-development drainage analysis at the subdivision and site plan stage of the development process. The City’s Land Development Code and Drainage Criteria Manual require that the Applicant demonstrate through engineering analysis that the proposed development will have no identifiable adverse impact on surrounding properties.

Environmental

No comments.

Fire

No comments.

Parks and Recreation

Parkland dedication will be required at the time of subdivision or site plan application for new residential units, per City Code § 25-1-601, as amended. The proposed development meets the criteria for land dedication in City Code Title 25, Article 14. As such, a partial land dedication will be required, as well as any remaining fees in-lieu.

The area is currently park deficient, defined as being outside walking distance to existing parks. The development as proposed will require parkland dedication for the new residential units that will serve the neighborhood being created through this PDA. Parks and Recreation Department (PARC) is open to working with the applicant to find suitable land that would work toward satisfying the requirement at time of permitting (whether subdivision or site plan). This land could be a neighborhood park for the 1000+ units proposed, improve neighborhood connectivity to and through the site for the existing neighbors, be a combination of these, or another idea to which PARC agrees.

Should there be any remaining fees in-lieu, those fees shall be used toward park investments in the form of land acquisition and/or park amenities within the surrounding area, per the Parkland Dedication Operating Procedures § 14.3.11 and City Code § 25-1-609.

Note that residential units that are certified affordable and income-restricted are exempt from the parkland dedication requirements per City Code § 25-1-601(D)(4).

Please contact this reviewer: ann.desanctis@austintexas.gov in advance of site plan or subdivision applications. At the applicant's request, PARC can provide an early determination letter of the requirements as stated in this review.

Site Plan

Zoning Cases

1. Site plans will be required for any new development other residential only project with up to 4 units.
2. Any new development is subject to Subchapter E. Design Standards and Mixed Use. Additional comments will be made when the site plan is submitted.

Transportation

ZONING REVIEW COMMENTS

In lieu of a ZTA, please amend the existing TIA associated with SP-2021-0109C. LDC 25-6, TCM 10.5.0. (*please see TIA Compliance Memo – Exhibit D*).

The Austin Strategic Mobility Plan (ASMP) calls for 116 feet of right-of-way for Duval. It is recommended that 58 feet of right-of-way from the existing centerline should be dedicated according to the ASMP with the first subdivision or site plan application. [LDC 25-6-51 and 25-6-55].

The Austin Strategic Mobility Plan (ASMP) calls for 58 feet of right-of-way for Duval. It is recommended that 29 feet of right-of-way from the existing centerline should be dedicated according to the ASMP with the first subdivision or site plan application. [LDC 25-6-51 and 25-6-55].

The adjacent street characteristics table is provided below:

Name	ASMP Classification	ASMP Required ROW	Existing ROW	Existing Pavement	Sidewalks	Bicycle Route	Capital Metro (within ¼ mile)
Duval Rd	Level 2/ Collector	116'	Varies	76'	Yes	Yes	Yes
West Cow Path	Level 1/ Local	58'	78'	45'	Portions of W Cow path have sidewalk	No	Yes
Research Blvd. Svr. SB	Level 4/ Major Arterial	NA TXDOT	338' (full 183 cross section)	40' (service road)	No	No	Yes

Water Utility

No comments on zoning change. Please note additional information below:

Due to a significant increase in total LUEs that will occur with the proposed development agreement, revisions to previously approved Service Extension Requests (SER) will be required to provide service to this lot. For more information pertaining to the Service Extension Request process and submittal requirements contact the Austin Water SER team at ser@austintexas.gov.

Previously approved development plans constructing public infrastructure per previous SERs may be constructing infrastructure not adequately sized to serve the future development. It is recommended to contact the SER team ASAP to resolve prior to construction.

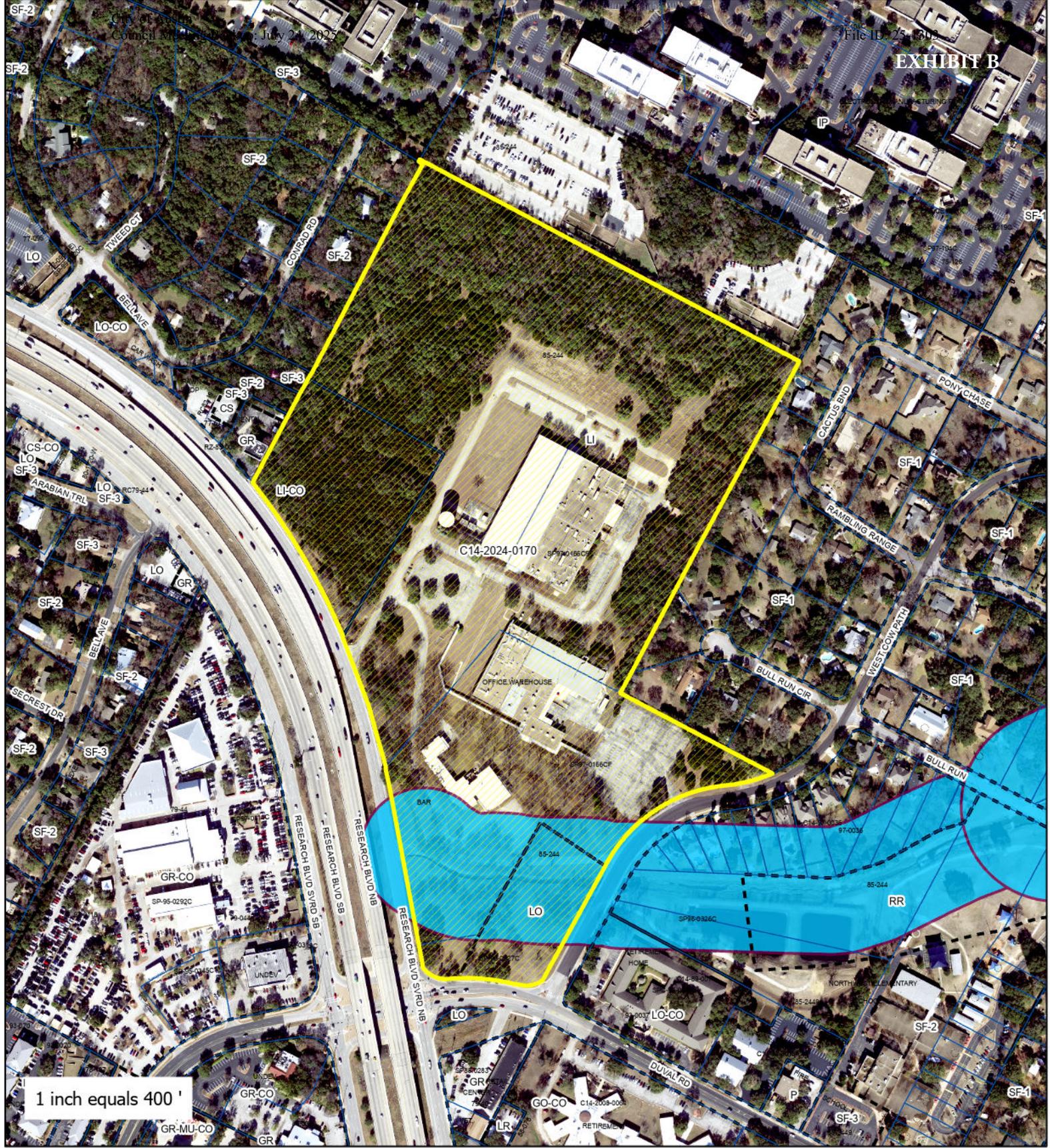
FYI: The landowner intends to serve the site with City of Austin water and wastewater utilities. The landowner, at own expense, will be responsible for providing any water and wastewater utility improvements, offsite main extensions, utility relocations and or abandonments required by the land use. The water and wastewater utility plan must be reviewed and approved by Austin Water for compliance with City criteria and suitability for operation and maintenance.

All new development within the proposed project will be subject to City Code Chapter 25-9 and any future amendments relating to reclaimed water connections, water benchmarking, and mandatory onsite water reuse.

All water and wastewater construction must be inspected by the City of Austin. The landowner must pay the City inspection fee with the utility construction. The landowner must pay the tap and impact fee once the landowner makes an application for a City of Austin water and wastewater utility tap permit.

INDEX OF EXHIBITS TO FOLLOW

- A. Zoning Map
- B. Aerial Map
- C. Applicant's Request Letter
- D. TIA Compliance Memo
- E. Comments from Interested Parties



1 inch equals 400'



-  SUBJECT TRACT
-  ZONING BOUNDARY
-  PENDING CASE
-  CREEK BUFFER

Duval PDA

ZONING CASE#: C14-2024-0170
 LOCATION: 5501 Duval Rd., and 11705,
 11755 1/2, 11757 1/2, 11781 1/2, 11785 1/2,
 11801 1/2 Research Blvd. and 11900,
 11900 1/2, 11902 1/2, 11904 1/2 W. Cow Pth
 SUBJECT AREA: 57.21 Acres
 MANAGER: Sherri Sirwaitis



This product has been produced by the Housing and Planning Department for the sole purpose of geographic reference. No warranty is made by the City of Austin regarding specific accuracy or completeness.

ARMBRUST & BROWN, PLLC

ATTORNEYS AND COUNSELORS

100 CONGRESS AVENUE, SUITE 1300
AUSTIN, TEXAS 78701-2744
512-435-2300

FACSIMILE 512-435-2360

Richard T. Suttle, Jr.
(512) 435-2300
rsuttle@abaustin.com

November 14, 2024

Lauren Middleton-Pratt
Director, City of Austin Planning Department
City of Austin Permitting and Development Center
6310 Wilhelmina Delco Drive
Austin, Texas 78752

Re: Rezoning Application for property located at 5501 Duval Road, 11705, 11755, 11755 1/2, 11757 1/2, 11781 1/2, 11785 1/2, and 11801 1/2 Research Boulevard SVRD NB, 11900, 11900 1/2, 11902 1/2, and 11904 1/2 West Cow Path (the "Application")

Dear Mrs. Middleton-Pratt:

This firm represents and this Application is submitted on behalf of Karlin Duval, LLC (the "Landowner"), to rezone ± 57.21 acres of land located at 5501 Duval Road, 11705, 11755, 11755 1/2, 11757 1/2, 11781 1/2, 11785 1/2, and 11801 1/2 Research Boulevard SVRD NB, 11900, 11900 1/2, 11902 1/2, and 11904 1/2 West Cow Path (the "Property"). The Property is currently zoned Limited Industrial Services ("LI"), Limited Industrial Services – Conditional Overlay Combining District ("LI-CO") and Limited Office District ("LO") and was previously developed as a manufacturing plant for 3M which has since been demolished with the exception of a $\pm 20,000$ square foot office building.

The Landowner originally intended to redevelop the property as a commercial office park with approximately 1.5 million square feet of office space and 10,000 square feet of retail. A site plan for 183 & Duval Buildings 1 & 5 was submitted and approved under Case No. SP-2021-0109C for the construction of approximately 490,000 square feet of office with two parking structures and associated infrastructure (the "Site Plan"). A Traffic Impact Analysis was submitted and approved with the Site Plan for the anticipated development of ± 1.5 million square feet of office and $\pm 10,000$ square feet of retail.

Due to changes in the market conditions the Landowner is seeking to rezone the Property from LI, LI-CO, and LO to Limited Industrial Services – Planned Development Area Combining District ("LI-PDA") to allow for the construction of approximately 1,200 multi-family units and 30,000 square feet of retail/restaurant uses. The modified development regulations proposed with this Application are provided in Exhibit "A" and attached hereto for reference.

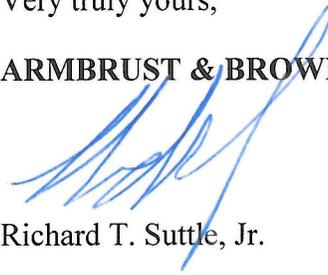
ARMBRUST & BROWN, PLLC

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Thank you in advance for your time and consideration of this zoning request. If you have any questions or need additional information, please do not hesitate to contact me at 512-435-2300.

Very truly yours,

ARMBRUST & BROWN, PLLC



Richard T. Suttle, Jr.

cc: Joi Harden, City of Austin
Amanda Hendrix, Armbrust & Brown, PLLC
Amanda Morrow, Armbrust & Brown, PLLC
Joe Prochot, Karlin Real Estate
Mike McGlashan, Karlin Real Estate

EXHIBIT "A"

PDA SITE DEVELOPMENT STANDARDS

Section 1. Applicable Site Development Regulations

- A. Unless otherwise modified herein development of the Property shall comply with applicable City of Austin rules, regulations and ordinances.
- B. If there is a conflict between this Ordinance and applicable City of Austin rules, regulations and ordinances, this Ordinance including the Exhibits shall control.

Section 2. Authorized Uses

- A. All Limited Industrial (LI) uses are permitted uses of the Property, except as set forth in Subsection B of this Section. The following are additional permitted uses:

Multi-Family Residential	Townhouse Residential
Condominium Residential	Performance Venue
Cocktail Lounge	Outdoor Entertainment
Research Assembly Services	Research Testing Services
Research Warehousing Services	Club or Lodge

- B. The following uses are prohibited as principal uses of the Property:

Automotive Repair Services	Building Maintenance Services
Campground	Commercial Off-Street Parking
Convenience Storage	Drop-Off Recycling Collection Facility
Funeral Services	Monument Retail Sales
Pedicab Storage and Dispatch	Scrap and Salvage
Vehicle Storage	Recycling Center
Resource Extraction	Railroad Facilities

Section 3. Site Development Regulations

- A. Base District Regulations

- 1) Development of the Property shall conform to the site development regulations authorized for the Limited Industrial Services (LI) district as set forth in the City Code, except as provided for in this Ordinance.
- 2) The following development regulations shall apply to the Property:
 - a) The minimum lot size is 5,750 square feet.
 - b) The minimum lot width is 50 feet.

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- c) A 50-ft minimum setback shall be required from property zoned SF-5 or more restrictive. There is no minimum interior side yard, rear yard, front yard, or street yard setbacks.
- d) The maximum height of a building or structure is 60-ft plus additional height limit exceptions in Section 25-2-531 (*Height Limit Exceptions*).
- e) The maximum impervious cover and building coverage is set forth by the applicable watershed limitation.
- f) The maximum floor-to-area ratio is 2:1.
- g) There are no minimum site area requirements.



Technical Memorandum

To: Ramin Komeili, P.E.
Austin Jones, P.E.
Mustafa Wali
City of Austin Permitting & Development Center
6310 Wilhelmina Delco Drive, Austin, TX 78752

From: Aditya Jatar (AJ), P.E.
BOE

File: 100235

Date: April 7, 2025

Regarding: TIA Compliance – 183/Duval Site – SP-2021-0109C

The following memorandum has been prepared in reference to the proposed development of US 183 and Duval site located at the northeast corner of US Highway 183 and Duval Road in Austin, Travis County, Texas. The TIA for the proposed site has already been completed and approved by City of Austin on February 8, 2022. The US 183 and Duval TIA approval memorandum has been included as **Attachment 1**.

The location of the proposed development is shown in **Figure 1**.

Figure 1 – Proposed US 183/Duval Site Location



April 7, 2025

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Since the TIA was approved and taking into consideration the current market conditions, the proposed site is to be rezoned for alternate land uses other than what were assumed as part of the approved TIA. Based on discussion with City of Austin, this TIA compliance memo includes comparison of the site trips between the original TIA land uses and recent updated proposed land uses, updated construction phasing of the identified improvements, and design and construction of the improvements identified in **Table 6** shall be part of the site development application during the phase that they are associated with as deemed suitable for updated land uses. The updated TIA Determination worksheet corresponding to updated land uses has been included as **Attachment 2**.

The previously approved US 183/Duval TIA was performed utilizing the **ITE Trip Generation Manual, 10th Edition** to establish the trip generation for the originally proposed land uses. **Table 1** below summarizes the unadjusted trip generation documented within the US 183/Duval TIA. **Table 2** below summarizes the unadjusted trip generation per phase documented within the US 183/Duval TIA. The total allowed TDM reductions per the approved TIA scope is 4.5 percent (4.5%), which is stated in Table 1 of the TIA Final Memo dated February 8, 2022, and is included as **Attachment #1** with this compliance memo. Please note that the trips provided in all the Tables within this compliance memo are unadjusted trips which are typically tallied for TIA compliance based on prior TIA compliance memos submitted to the City of Austin. The Adjusted Trips are also provided in Table 1 of the TIA Final Memo.

Table 1 – Approved TIA - Unadjusted Trip Generation

Phase	Build Year	ITE Code	Land Use	Size	24-Hour Daily Volume	AM Peak Hour			PM Peak Hour			
						Enter	Exit	Total	Enter	Exit	Total	
PROPOSED												
		710	General Office	1,521,738	s.f.	14,880	1,253	204	1,457	242	1,271	1,513
		820	Retail	10,000	sf	1,256	97	60	157	48	51	99
TOTAL PROPOSED				1,531,738	s.f.	16,136	1,350	264	1,614	290	1,322	1,612

Table 2 – Approved TIA – Unadjusted Trip Generation – Per Phase

Phase	Build Year	ITE Code	Land Use	Size	24-Hour Daily Volume	AM Peak Hour			PM Peak Hour			
						Enter	Exit	Total	Enter	Exit	Total	
PROPOSED												
1	2022	710	General Office	271,739	s.f.	2,798	241	39	282	47	247	294
2	2023	710	General Office	271,739	s.f.	2,798	241	39	282	47	247	294
3	2024	710	General Office	271,739	s.f.	2,798	241	39	282	47	247	294
4	2025	710	General Office	271,739	s.f.	2,798	241	39	282	47	247	294
5	2026	710	General Office	217,391	s.f.	2,254	198	32	231	38	200	238
6	2027	710	General Office	217,391	s.f.	2,254	198	32	231	38	200	238
7	2028	820	Retail	10,000	sf	1,256	6	4	9	47	51	99
TOTAL PROPOSED				1,531,738	s.f.	16,956	1,367	226	1,599	312	1,440	1,752



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The **ITE Trip Generation Manual, 11th Edition** is used to establish the trip generation for the current updated proposed land uses. **Table 3** below summarizes the unadjusted trip generation for the current updated proposed land uses. **Table 4** below summarizes the unadjusted trip generation per phase for the current updated proposed land uses. The breakdown of the trip generation summary for proposed land uses shown within Table 3 and Table 4 are included as **Attachment #3** for reference.

Table 3 – Unadjusted Trip Generation Updated Land Uses

Phase	Build Year	ITE Code	Land Use	Size		24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
							Enter	Exit	Total	Enter	Exit	Total
PROPOSED												
		221	MF Mid Rise	1,200	du	5,678	119	397	516	286	182	468
		932	HT Restaurant	30,000	sf	3,216	158	129	287	166	106	272
TOTAL PROPOSED				31,200	s.f.	8,894	277	526	803	452	288	740

Table 4 – Unadjusted Trip Generation Updated Land Uses – Per Phase

Phase	Build Year	ITE Code	Land Use	Size		24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
							Enter	Exit	Total	Enter	Exit	Total
PROPOSED												
1	2026	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
2	2028	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
3	2030	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
4	2032	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
4	2032	932	HT Restaurant	30,000	sf	3,216	158	129	287	166	106	272
TOTAL PROPOSED				31,200	s.f.	8,894	277	526	803	452	288	740

When comparing the trips related to the originally approved TIA (**Table 1**) to what is currently proposed (**Table 3**), it can be seen and confirmed that the total site trips for the updated land uses are lower than the total site trips considered as part of the originally approved TIA and remain in compliance with the approved TIA. **Table 5** below provides a summary of the remaining trips available for the US 183/Duval tract.

Table 5 – Net Remaining Trips

Trip Generation Comparison	24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
		Enter	Exit	Total	Enter	Exit	Total
Total Trips TIA	16,136	1,350	264	1,614	290	1,322	1,612
Total Trips for the Proposed Land Uses	8,894	277	526	803	452	288	740
Net Remaining Trips	7,242	1,073	(262)	811	(162)	1,034	872



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The previously approved TIA considered US 183/Duval site to be developed primarily as an Office land use in multiple phases (Phase 1-Phase 6) as seen within **Table 2**. At this time, the US 183/Duval site is to be developed primarily as a Multi-Family land use in multiple phases (Phase 1- Phase 4) as seen within **Table 4**. The change in land uses results in opposite directional flow of trips to/from the site with switching of the AM and PM peak hour trips following the changes in major land use from Office to Multi-Family. The improvements identified as part of the original approved TIA memo (**See Table 6**) have been determined adequate to mitigate and support the total number of AM (1,614) and PM (264) site trips based on the major land use (Office) shown within **Table 2**. As the total number of AM (803) and PM (740) site trips based on the new proposed major land use (Multi-Family) shown within **Table 4** are lower than trips shown with **Table 2**, the improvements identified shall continue to be adequate to mitigate and support the total AM (803) and PM (740) of the proposed new major land use (Multi-Family).

Summary of Improvements

Historically, a Fee-in-lieu contribution to the City of Austin would have been made for the improvements identified in Table 2a, before the site development permit was issued. With the passage of the Street Impact Fee (SIF) Ordinance, the Transportation Department will not collect this as a fee-in-lieu payment now but will collect it in the form of SIF with each site plan as appropriate, as required by the SIF Ordinance. SIF is a process for funding the public roadway infrastructure to meet the needs of new development. SIF is the maximum allowable roadway impact fee that could be assessed by the COA. It is a technical calculation that quantifies the incremental cost of the impact of the proposed development on the street infrastructure. SIF is a charge assessed on new development to pay for the construction/expansion of roadway facilities that will benefit the proposed development area.

Per the approved US 183/Duval TIA Approval Memo, several mitigation measures were identified. The mitigation measures identified for study area intersections were split into two components namely (i) design and construction improvements tied to phasing breakdown associated with land uses within the original approved TIA and (ii) SIF fees to be collected as discussed earlier. **Table 6** below provides a summary of the recommended improvements and their breakdown into construction vs SIF as documented withing the US 183/Duval TIA Approval Memo.

Design and construction of the improvements identified in **Table 6** shall be part of the site development application during the phase that they are associated with. The City of Austin will confirm that these improvements have been constructed and completed in Phase 2 of the project during the Site Plan review of Phase 2 construction. If these improvements have not been constructed by the time the Phase 3 Site Plan application is submitted, the City of Austin reserves the right to deny any further applications for site development permit until the improvements listed have been completed.



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Table 6 – Recommended Improvements

Intersection	Recommended Improvements		
US 183 NBFR & Duval Road	Extend Westbound Right-turn Lane to 300 feet (250 feet storage/50 feet taper)		
	Traffic Signal Timing Modifications		
Mopac SBFR & Duval Road	Extend Eastbound Right-turn Lane to 800 feet (700 feet storage/100 feet taper)		
	Install Southbound Left-turn Lane		
	Install Southbound Right-turn Deceleration Lane		
	Signal Infrastructure Modifications		
	Traffic Signal Timing Modifications		
Mopac NBFR & Duval Road	Install Westbound Right-turn Deceleration Lane		
	Signal Infrastructure Modifications		
	Traffic Signal Timing Modifications		
Duval Road & Angus Street	Pedestrian Hybrid Beacon		
Intersection/Roadway	Design and Construction Improvements (Required)	Required Improvements Implementation Phase & Year (Approved TIA Memo ¹)	Required Improvements Implementation Phase (TIA Compliance Memo ²)
Duval Road (W Cow Path-US 183 NBFR)	400 feet of protected Bike Lane on Duval Road along property frontage per ASMP	Phase 3 (2024)	Phase 2 (2028)
W Cow Path & Duval Road	Signalize as Continuous Green T-intersection		
<i>¹ Reference approved TIA Final Memo (SP-2021-0109C) (Dated February 8, 2022) within Appendix A.1 identifying Phase 3 (Year 2024) for implementation timeline of the required improvements identified above.</i>			
<i>² Based on the analysis presented within this TIA compliance memo, Phase 2 (Year 2028) is the comparable implementation timeline of the required improvements identified above based on adjustment made to the proposed land uses.</i>			
<i>The US 183/Duval site shall be responsible for contributing towards the Street Impact Fee (SIF) program. The SIF will be collected at the time of building permit tied to each phase of site development. The eligible SIF Fees will be determined following guidelines available as part of City of Austin SIF Program.</i>			

The US 183/Duval TIA Approval Memo considers Phase 3 (2024) for the implementation of the design and construction improvements identified under **Table 6**. The final approved plans for proposed signal at Duval Road & West Cow Path are included as Attachment 4.

To determine the implementation year of the design and construction improvements identified under **Table 6** as related to the updated land use, the percentage of site trips completed under Phase 3 (2024) is calculated as shown within **Table 7** below.



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Table 7 – Percentage Trips for Phase 3 (2024) Original TIA

Trip Generation Comparison	24-Hour Daily Volume	AM Peak Hour			PM Peak Hour			Average	
		Enter	Exit	Total	Enter	Exit	Total		
Phase 3 2024 Site Trips	8,394	724	118	846	141	742	883	-	
Total Proposed Site Trips	16,956	1,367	226	1,599	312	1,440	1,752		
Percentage of total proposed trips	50%	53%	52%	53%	45%	51%	50%	Average	51%

The percentage difference between the daily trips of the original TIA and proposed site trips based on updated land uses is **55%**. Adjusting the percentage applicable to Phase 3 (2024) calculated within Table 7 results in **28%**. **Table 8** and **Table 9** below show the calculations performed to determine the correct implementation year to design and of the design and construction improvements identified under **Table 6** as related to trips from updated land uses.

Table 8 – Percentage Factor Applicable to Phase 3 (2024) Original TIA

Description	ADT		Percentage
Percentage difference of Daily Trips between Original TIA and Updated Land Uses	16,956	8,894	55%
Adjustment of percentage applicable to Phase 3 (2024)	51% * 55%		28%

Table 9 – Percentage Factor Applied to Trips for Proposed Site (Updated Land Uses)

Comparison Scenarios	24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
		Enter	Exit	Total	Enter	Exit	Total
Total Proposed Site Trips	8,894	277	526	803	452	288	740
Percentage of total proposed trips	2,490	78	147	225	127	81	207
	28%	28%	28%	28%	28%	28%	28%

Table 10 below shows the summary of trips associated with updated land uses that meet the 28% percentage factor from **Table 4**. It can be seen that Applying **28%** to Table 4 results in identifying **Phase 2 (2028)** to be comparable year for implementation of implementation of the design and construction improvements identified under **Table 6** considering that **Phase 2 (2028)** exceeds the threshold calculated within **Table 9**.



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Table 10 – Implementation Year for Construction Improvements based on Updated Site Trips

Phase	Build Year	ITE Code	Land Use	24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
					Enter	Exit	Total	Enter	Exit	Total
Phase 2	2028	221	MF Mid Rise	2,839	60	198	258	142	90	234

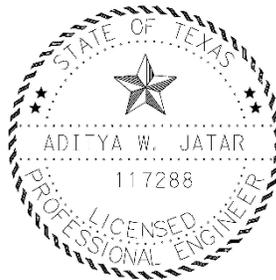
Please let us know if you have any questions or need any additional information.

Sincerely,

BOE CONSULTING SERVICES, LLC
Texas Engineering Firm No. F-19220

April 7, 2025

Aditya Jatar (AJ), P. E
Project Manager
Phone: 737.301.2311
aj@bo-engineering.com



- 1: US 183/Duval TIA Approval Memo (2022-02-08)**
- 2: COA Signed TDW (2024-11-05)**
- 3: Trip Generation Calculations (Proposed Land Uses)**
- 4: Duval/West Cow Path Approved Signal Plans (2024-03-06)**

01
US 183/DUVAL TIA APPROVAL MEMO
2022-02-08



MEMORANDUM

Date: February 8, 2022
To: Bobak J Tehrany, P.E. (BOE)
CC: Curtis Beaty, P.E.
Sangeeta Jain, AICP

Reference: US 183 and Duval Office Complex – TIA Final Memo (SP-2021-0109C)

Summary of the Transportation Impact Analysis (TIA):

The Transportation Development Services Division (within the Austin Transportation Department, ATD) reviewed the December 2020 traffic impact analysis (TIA) report (received December 18, 2020) regarding the “US 183 and Duval Office Complex” development, prepared by BOE. The TIA was submitted with the Site Plan application, concurrent with a zoning application for a small subset of the site. The zoning case was contested by the neighborhood which caused there to be some uncertainty regarding its likelihood of being approved by Council. Due to this fact, transportation staff were not able to continue with the review of the Site Plan application until the zoning case was finalized, which caused the delay between receiving the TIA and this approval memo. The proposed development will consist of 1,521,738 SF of General Office and 10,000 SF of retail and is located at 11705 Research Blvd, as shown in Figure 1. The development is anticipated to complete construction in 2028.

The following is a summary of the review findings and recommendations:

1. Historically, a Fee-in-lieu contribution to the City of Austin would have been made for the improvements identified in Table 2a, totaling **\$318,750**, before the site development permit was issued. With the passage of the Street Impact Fee (SIF) Ordinance, the Transportation Department will not collect this as a fee-in-lieu payment now but will collect it at the time of building permit, as required by the SIF Ordinance.
2. Design and construction of the improvements identified in Table 2b shall be part of the site development application during the phase that they are associated with. ATD will confirm that these improvements have been constructed during the Site Plan review of Phase 3 construction. If these improvements have not been constructed by the time the Phase 4 application is submitted, ATD reserves the right to deny any further applications for site development permit until the improvements listed have been completed.
3. The applicant is required to achieve a vehicle trip reduction as described in Table 1. The applicant commits to implement the Transportation Demand Management measures to achieve the identified reduction.

4. Development of this property should not vary from the approved uses or deviate from the approved intensities and estimated traffic generation assumptions within the finalized TIA document, including land uses, trip generation, trip distribution, traffic controls, driveway locations, and other identified conditions. Any change in the assumptions made to the approved TIA document that this memo references shall be reviewed by ATD and may require a new or updated TIA/addendum.
5. A final copy of the TIA should be delivered to ATD (digitally) to act as the copy of record.
6. City staff reserves the right to reassign any or all the above monies to one or more of the identified improvements as it deems appropriate.
7. The findings and recommendations of this TIA memorandum remain valid until five (5) years from the date of the traffic counts in the TIA or the date of this memo, whichever comes first, after which a revised TIA or addendum may be required.
8. Street Impact Fee Ordinances 20201220-061 and 20201210-062 have been adopted by City Council and are effective as of December 21, 2020. The City shall start collecting street impact fees with all building permits issued on or after June 21, 2022. For more information, please visit the Street Impact Fee website [www.austintexas.gov/streetimpactfee].

Figure 1 - Site Location Map



Site Location and Existing Conditions:

The site is currently occupied by a 201,645 SF General Office complex. The proposed mixed-use development will utilize two (2) driveways for site access, as detailed below:

- Driveway A (Private) - Full access along US 183 Northbound Frontage Rd
- Driveway B (Private) - Full access along W Cow Path (northern driveway)
- Driveway C (Private) - Full access along W Cow Path (southern driveway)

Assumptions:

1. A 4.5% trip reduction was allowed due to the TDM measures proposed by the applicant.
2. Based on TxDOT AADT volume data, a one (1) percent annual growth rate was assumed to account for the increase in background traffic.
3. No background projects were identified in the vicinity of the site.
4. The project will be completed in seven (7) phases:
 - Phase 1 (2022) – 271,739 SF General Office
 - Phase 2 (2023) – 271,739 SF General Office
 - Phase 3 (2024) – 271,739 SF General Office
 - Phase 4 (2025) – 271,739 SF General Office
 - Phase 5 (2026) – 271,739 SF General Office
 - Phase 6 (2027) – 271,739 SF General Office
 - Phase 7 (2028) – 10,000 SF Retail

Proposed Conditions:

Trip Generation and Land Use

Based on the Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition), the development would generate approximately 16,136 unadjusted average daily trips (ADT) upon final build-out.

Due to the number of vehicle trips and the anticipated traffic load on the roadway network, the applicant committed to a Transportation Demand Management (TDM) Plan to reduce their site vehicle trips by 4.5%. Table 1 shows the adjusted trip generation after applying TDM reductions and reductions for existing site trips.

Transportation Demand Management (TDM)

The applicant has committed to a 4.5% TDM reduction to meet vehicle trip reduction targets. In the TDM plan, the applicant identified several measures that could be implemented with the site to achieve the vehicle trip reduction.

Table 1: Trip Generation						
Proposed Land Use		Size / Unit		24-Hour Two Way Volume	AM Peak Hour	PM Peak Hour
710	General Office	1,521,738	SF	14,880	1,457	1,513
820	Retail	10,000	SF	1,256	10	99
Total Unadjusted Trips				16,136	1,467	1,612
<i>TDM Reduction (4.5%)</i>				<i>(726)</i>	<i>(66)</i>	<i>(73)</i>
<i>Existing Trips</i>				<i>(2,095)</i>	<i>(216)</i>	<i>(222)</i>
Total Adjusted Trips				13,315	1,185	1,317

The applicant identified the following key TDM measures that may be used to achieve the 4.5% TDM reduction target:

- Bicycle Parking
- Showers & Lockers
- Bicycle Repair Station
- Telecommuting
- TMA Membership

No specific TDM measures are currently being proposed, however, individual site plans and phases of construction will provide proposals for specific TDM measures the developer will enact. Each phase will need to provide documentation and proof in any memos claiming compliance with this TIA memo to support the 4.5% TDM reduction being granted. If proper TDM measures can be proven, the 4.5% reduction will be upheld, otherwise, individual proposed phases of development included under this memo may be subject to the SIF without any TDM reduction. The determination will be made with each site plan submitted on this tract and may require additional mitigation or fee if an acceptable TDM plan cannot be provided.

Summary of Recommended Improvements

Table 2a: Recommended Improvements (Fee-in-Lieu)		
Intersection	Improvement	Estimated Total Cost¹
US 183 NBFR and Duval Rd	Extend Westbound Right-Turn Lane to 300ft (250-foot storage/50-foot taper)	\$187,500
	Traffic Signal Timing Modifications	\$6,250
Mopac SBFR & Duval Road	Extend Eastbound Right-Turn Lane to 800ft (700-foot storage/100-foot taper)	\$187,500
	Install Southbound Left Turn Lane	\$312,500
	Install Southbound Right-Turn Deceleration Lane	\$187,500
	Signal Infrastructure Modifications	\$125,000
	Traffic Signal Timing Modifications	\$6,250
Mopac NBFR & Duval Road	Install Westbound Right-Turn Deceleration Lane	\$187,500
	Signal Infrastructure Modifications	\$125,000
	Traffic Signal Timing Modifications	\$6,250
Duval Road & Angus Street	Pedestrian Hybrid Beacon	\$150,000
Total:		\$1,481,250¹
Table 2b: Required Improvements (Construction)		
Duval Road (W Cow Path – US 183 NBFR)	400 ft of protected Bike Lane on Duval Rd along property frontage per ASMP	
W Cow Path & Duval Road	Signalize as Continuous Green T-intersection	

¹ With the implementation of Street Impact Fees, ATD will collect this money as a SIF, not a separate payment as a Fee-In-Lieu.

If you have any questions or require additional information, please contact me at (512) 974-7136.

A handwritten signature in black ink that reads "Nate Aubert". The signature is written in a cursive style with a large, stylized "N" and "A".

Nathan Aubert, P.E.

Austin Transportation Department

02
SIGNED TIA DETERMINATION WORKSHEET
2024-11-05





Traffic Impact Analysis (TIA) Determination Worksheet

Applicant must complete this worksheet except where noted for TPW Staff. Please submit completed worksheet to the TIA Determination Worksheet portal (<https://atd.knack.com/development-services#services/traffic-impact-analysis-determination/>) for review and signature.

Project Name: _____

Location: _____

Applicant: _____ Telephone No: _____

Application Type: Dev. Assessment (Zoning) Zoning Site Plan
Indicates determination is optional Dev. Assessment (Site Plan) Concept Site Plan*

By checking the box below, the applicant acknowledges that City Council has adopted a Street Impact Fee (SIF) program effective December 21, 2020, and that street impact fees will be assessed for any building permit pulled on or after June 21, 2022. For more information on the Street Impact Fee program, please visit www.austintexas.gov/department/street-impact-fee

Applicant acknowledgment of Street Impact Fee program

EXISTING:

FOR TPW STAFF USE ONLY

Tract Number	Tract Acres	Units**	Zoning	Land Use	I.T.E. Code	Trip Rate	Trips Per Day

Please note that existing trip generation in the above table is applicable only to this worksheet. Existing trip generation for use in transportation studies and SIF calculations shall be determined separately.

PROPOSED:

FOR TPW STAFF USE ONLY

Tract Number	Tract Acres	Units**	Zoning	Land Use	I.T.E. Code	Trip Rate	Trips Per Day

**Applicable based on land use (e.g., dwelling units for residential, building square footage for commercial, etc.)

ABUTTING ROADWAYS:

Street Name	Proposed Access (Y/N)	Proposed Number of Driveways	ASMP Street Level



FOR TPW STAFF USE ONLY

- A Transportation Demand Management (TDM) Plan is required. For more information on the contents required in a TDM Plan, please refer to Section 10 of the Transportation Criteria Manual (TCM) or contact a Lead Development Review Engineer.
- A traffic impact analysis is required. The consultant preparing the study must contact a Lead Development Review Engineer to discuss scoping requirements prior to beginning the study. Please see below for the type of study required; for more information on each study, please refer to Section 10 of the TCM.
 - Full TIA Transportation Assessment (TA) Zoning Transportation Analysis (ZTA) TIA Compliance
- A traffic impact analysis is NOT required. Traffic generated by the proposal does not exceed the thresholds established in the City of Austin Land Development Code (LDC). Mitigation per LDC 25-6-101 may still apply.
- The traffic impact analysis has been waived for the following reason:

- A neighborhood traffic analysis (NTA) is required per LDC 25-6-114. The applicant may have to collect current traffic counts. Please contact a Transportation Planner for information.

Reviewed By: Ramin Komeli Date: _____

NOTE: A TIA determination must be made prior to submittal of any Zoning or Site Plan application; therefore, this completed and reviewed worksheet must accompany any subsequent application for the identical project. Changes to the proposed project may require a new TIA determination. This worksheet will remain valid for 90 calendar days from the approval date above, after which a new TIA Determination Worksheet will be required.

03
TRIP GENERATION CALCULATIONS
PROPOSED LAND USED
US 183 & DUVAL



TRIP GENERATION SUMMARY EQ vs RATES (PROPOSED LAND USES) US 183 AND DUVAL

ITE Trip Generation Manual 11th Edition - Rate/Equation Table

ITE Code	Land Use	24-Hour Daily Volume Rate or Eq	AM Peak Hour			PM Peak Hour		
			% Ent	% Ext	Rate or Eq	% Ent	% Ext	Rate or Eq
221	MF Mid Rise	$T = 4.77(X) - 46.46$	23%	77%	$T = 0.44(X) - 11.61$	61%	39%	$T = 0.39(X) + 0.34$
932	HT Restaurant	107.2	55%	45%	9.57	61%	39%	9.05

*This table was populated utilizing the criteria set by the ITE Trip Generation Handbook defining when a rate is to be utilized versus an equation.

**SUMMARY OF TRIP GENERATION CALCULATIONS
(PROPOSED LAND USES)
US 183/DUVAL**

Phase	Build Year	ITE Code	Land Use	Size		24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
							Enter	Exit	Total	Enter	Exit	Total
PROPOSED												
		221	MF Mid Rise	1,200	du	5,678	119	397	516	286	182	468
		932	HT Restaurant	30,000	sf	3,216	158	129	287	166	106	272
TOTAL PROPOSED				31,200	s.f.	8,894	277	526	803	452	288	740

Trip Generation is calculated using ITE Trip Generation Handbook 11th Edition

Phase	Build Year	ITE Code	Land Use	Size		24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
							Enter	Exit	Total	Enter	Exit	Total
		221	MF Mid Rise	1,200	du	5,678	119	397	516	286	182	468
		221	MF Mid Rise	1	du	4.73	0.10	0.33	0.43	0.24	0.15	0.39
<i>Trip Rate per dwelling Unit for MF Mid Rise is calculated by dividing the ADT, AM Peak and PM Volumes by 1,200</i>												

Phase	Build Year	ITE Code	Land Use	Size		24-Hour Daily Volume	AM Peak Hour			PM Peak Hour		
							Enter	Exit	Total	Enter	Exit	Total
EXISTING												
-	-	160	Data Center	10,000	s.f.	10	1	0	1	0	1	1
TOTAL EXISTING				10,000	s.f.	10	1	0	1	0	1	1
PROPOSED												
1	2026	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
2	2028	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
3	2030	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
4	2032	221	MF Mid Rise	300	du	1,419	30	99	129	71	45	117
4	2032	932	HT Restaurant	30,000	sf	3,216	158	129	287	166	106	272
TOTAL PROPOSED				31,200	s.f.	8,894	277	526	803	452	288	740
<i>Trip Rate calculated per dwelling unit has multiplied by number of units for each phase to determine the ADT, AM and PM peak hour volumes for each Phase of MF Mid Rise</i>												



04
DUVAL/W COW PATH APPROVED SIGNAL PLANS
2024-03-06



871S-A(4)(100)(Y)	REFLECTORIZED TYPE I THERMOPLASTIC PAVEMENT MARKINGS 4 INCHES IN WIDTH, 100 MIL THICKNESS YELLOW IN COLOR (DASH)	LF	74
871S-A(6)(100)(W)	REFLECTORIZED TYPE I THERMOPLASTIC PAVEMENT MARKINGS 6 INCHES IN WIDTH, 100 MIL THICKNESS WHITE IN COLOR (SOLID)	LF	1213
871S-A(8)(100)(W)	REFLECTORIZED TYPE I THERMOPLASTIC PAVEMENT MARKINGS 8 INCHES IN WIDTH, 100 MIL THICKNESS WHITE IN COLOR (SOLID)	LF	90
871S-A(8)(100)(W)	REFLECTORIZED TYPE I THERMOPLASTIC PAVEMENT MARKINGS 8 INCHES IN WIDTH, 100 MIL THICKNESS WHITE IN COLOR (DOT)	LF	113
871S-D(A)(100)(W)	REFLECTORIZED TYPE I THERMOPLASTIC PAVEMENT MARKINGS, ARROW, 100 MIL THICKNESS, WHITE IN COLOR	EA	4
871S-E(12)(100)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 12 INCHES IN WIDTH, 100 MIL THICKNESS, WHITE IN COLOR	LF	68
871S-E(24)(100)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 24 INCHES IN WIDTH, 100 MIL THICKNESS, WHITE IN COLOR	LF	221
871S-E(4)(100)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 4 INCHES IN WIDTH, 100 MIL THICKNESS, WHITE IN COLOR (SOLID)	LF	564
871S-E(4)(100)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 4 INCHES IN WIDTH, 100 MIL THICKNESS, WHITE IN COLOR (BROKEN)	LF	60
871S-E(12)(100)(Y)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 12 INCHES IN WIDTH, 100 MIL THICKNESS, YELLOW IN COLOR (SOLID)	LF	102
871S-E(4)(100)(Y)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 4 INCHES IN WIDTH, 100 MIL THICKNESS, YELLOW IN COLOR (SOLID)	LF	1762
871S-E(4)(100)(Y)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 4 INCHES IN WIDTH, 100 MIL THICKNESS, YELLOW IN COLOR (DASH)	LF	74
871S-E(6)(100)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 6 INCHES IN WIDTH, 100 MIL THICKNESS, WHITE IN COLOR (SOLID)	LF	1213
871S-E(8)(100)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 8 INCHES IN WIDTH, 100 MIL THICKNESS, WHITE IN COLOR (DOT)	LF	90
871S-E(8)(100)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, 8 INCHES IN WIDTH, 100 MIL THICKNESS, WHITE IN COLOR (DASHED)	LF	113
871S-H(A)(W)	REFLECTORIZED TYPE II PAINT PAVEMENT MARKINGS, ARROW, WHITE IN COLOR	EA	4
874-A-4	ELIMINATING EXISTING PAVEMENT MARKINGS: 4 INCHES IN WIDTH	LF	1642
874-A-6	ELIMINATING EXISTING PAVEMENT MARKINGS: 6 INCHES IN WIDTH	LF	1260
874-A-8	ELIMINATING EXISTING PAVEMENT MARKINGS: 8 INCHES IN WIDTH	LF	144
874-A-12	ELIMINATING EXISTING PAVEMENT MARKINGS: 12 INCHES IN WIDTH	LF	38
874-A-24	ELIMINATING EXISTING PAVEMENT MARKINGS: 24 INCHES IN WIDTH	LF	70
SP432S-RP-2A	P.C. CONCRETE CURB RAMP WITH PAVERS (COMBINED)	EA	2
SP432S-RP-3	P.C. CONCRETE CURB RAMP WITH PAVERS (MEDIAN RAMP)	EA	1
SP874S-AS	ELIMINATING EXISTING PAVEMENT MARKINGS: SYMBOLS/WORDS	EA	4
SS0351-6033	FLEX PAVEMENT STRUCT REPAIR(13IN-22IN)	SY	310
SS0666-6056	REFL PAV MRK TY I(Y)(MED NOSE) (100 MIL)	EA	1
SS0682-6054	BACKPLATE W/REF BRDR(3 SEC)(VENT)ALUM	EA	4
SS0682-6055	BACKPLATE W/REF BRDR(4 SEC)(VENT)ALUM	EA	3
SS883-A	CITY POST SM CHANNELIZER POSE WHITE (INITIAL PLACEMENT)	EA	25
SS883-A	CITY POST SM CHANNELIZER POSE YELLOW (INITIAL PLACEMENT)	EA	96
SS883-E	CITY POST SM CHANNELIZER POST (REMOVAL)	EA	19
SS1004-PSM	RIGID ALUMINUM CONDUIT	EA	3
SS1040-T	RADAR VEHICLE DETECTION SYSTEM, 3 APPROACH INTERSECTION, COMPLETE IN PLACE	EA	1
SS1020-DTP	#12/2C (APS)	LF	2638
SS1020-5C	#14/5C (PEDESTRIAN HEADS)	LF	386
SS1020-7C	#14/7C (SIGNAL CABLE)	LF	155
SS1020-20C	#14/20C (MAIN SIGNAL CABLE)	LF	2049
SS1020-2CT	#10/2C (LUMINAIRE)	LF	455
SS1020-6G	#6 BARE (GROUNDING FOR ALL OTHERS)	LF	2497
SS1044-CCTV	CCTV CAMERA, COMPLETE IN PLACE	EA	1

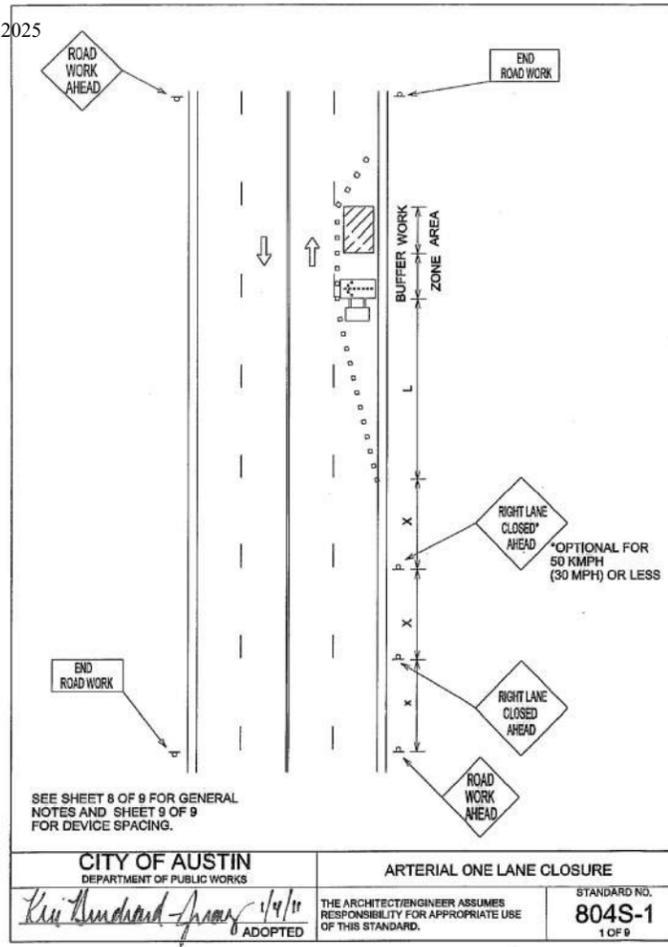
REV. NO.	BY	DATE	DESCRIPTION

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
SUMMARY OF QUANTITIES (2 OF 2)



NOTES	NAME	DATE
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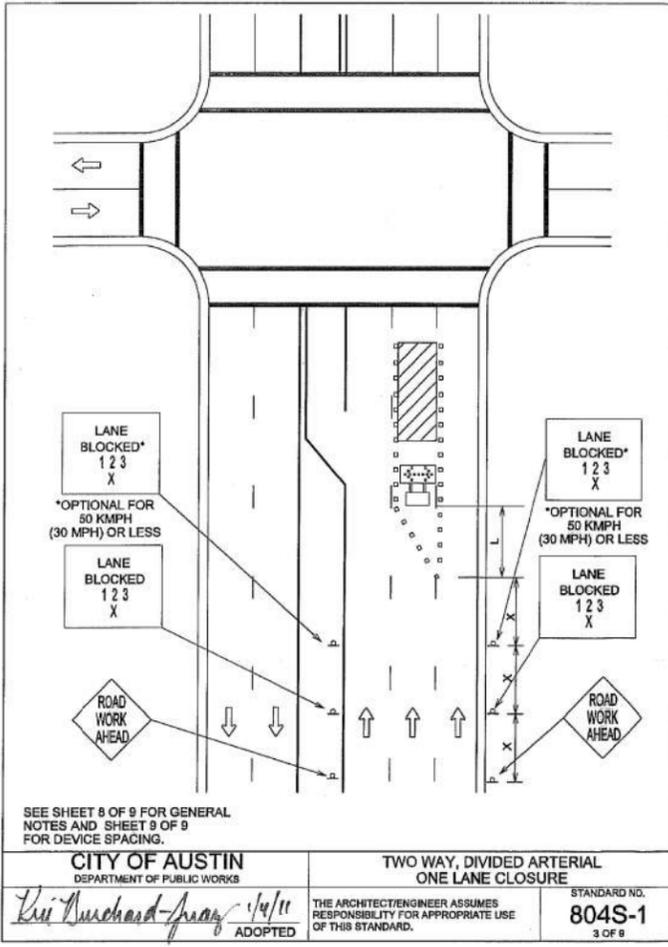


CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS
Kui Thurmond-Jones 4/4/11
ADOPTED

ARTERIAL ONE LANE CLOSURE

STANDARD NO. 804S-1
1 OF 9

THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.

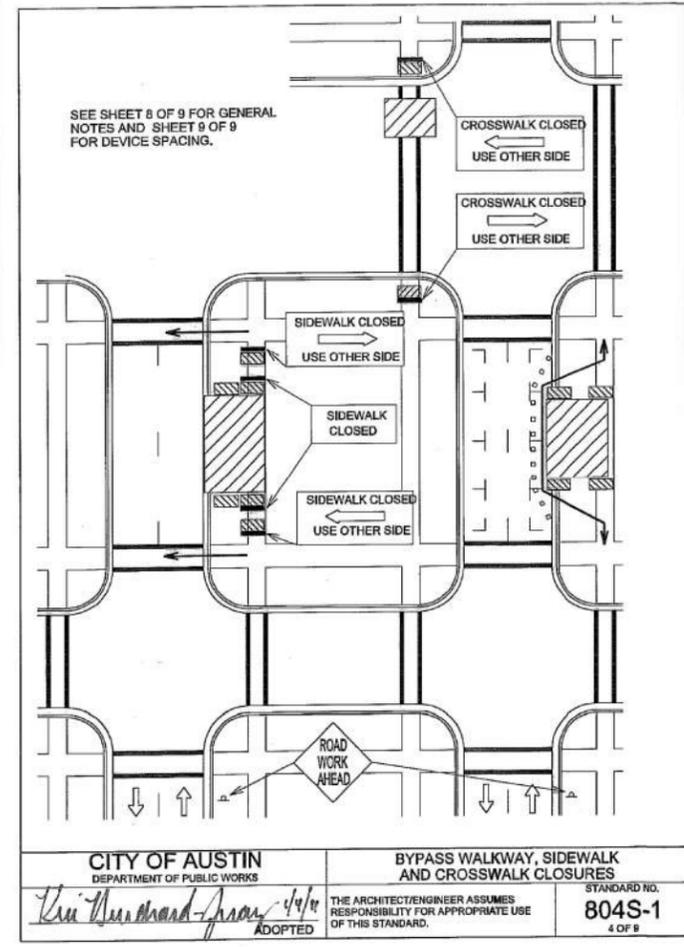


CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS
Kui Thurmond-Jones 4/4/11
ADOPTED

TWO WAY, DIVIDED ARTERIAL ONE LANE CLOSURE

STANDARD NO. 804S-1
3 OF 8

THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.



CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS
Kui Thurmond-Jones 4/4/11
ADOPTED

BYPASS WALKWAY, SIDEWALK AND CROSSWALK CLOSURES

STANDARD NO. 804S-1
4 OF 8

THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.

- ALL SETUPS SHALL BE IN ACCORDANCE WITH THE CURRENT ADDITION OF THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE CITY OF AUSTIN TRANSPORTATION CRITERIA MANUAL.
- TO DETERMINE APPROPRIATE DEVICES AND SIGN SIZES TO BE USED, REFER TO STANDARD 804S-5, SHEETS 5, 6 AND 7 OF 11.
- FOR INTERMEDIATE-TERM SITUATIONS, WHEN IT IS NOT FEASIBLE TO REMOVE AND RESTORE PAVEMENT MARKINGS, THE CHANNELIZATION MUST BE MADE DOMINANT BY USING A VERY CLOSE DEVICE SPACING. THIS IS ESPECIALLY IMPORTANT IN LOCATIONS OF CONFLICTING INFORMATION, SUCH AS WHERE TRAFFIC IS DIRECTED OVER A DOUBLE YELLOW CENTERLINE. IN SUCH LOCATIONS, A MAXIMUM CHANNELIZING DEVICE SPACING OF 3 m (10') IS REQUIRED.
- FOR LONG TERM STATIONARY WORK, ALL CONFLICTING PAVEMENT MARKINGS MUST BE REMOVED AND CENTERLINE STRIPING PROVIDED WHERE TWO WAY TRAFFIC IS IN ADJACENT LANES.
- FOR TEMPORARY PAVEMENT MARKING REQUIREMENTS SEE STANDARD 804S-3.
- FOR ONE-WAY AND MULTI-LANE ROADWAYS THE 'LANE BLOCKED' SIGN MAY BE USED IN LIEU OF THE 'LANE CLOSED AHEAD' SIGN. THE NUMBER OF DIGITS ON THE SIGN SHALL NOT BE GREATER THAN THE NUMBER OF LANES PRESENT ON THE ROADWAY. THE 'X' SHALL BE PLACED UNDER THE NUMBER OF LANE(S) BLOCKED.
- FOR FLAGGING OPERATION REQUIREMENTS SEE STANDARD 804S-2.
- CONTRACTOR SHALL PROVIDE SIDEWALK CLOSURES, CROSSWALK CLOSURES OR WALKWAY BYPASS WHEREVER PEDESTRIAN MOVEMENTS ARE AFFECTED BY CONSTRUCTION ACTIVITIES. ALL SIDEWALKS AND CROSSWALKS SHALL BE ACCESSIBLE WHEN CONTRACTOR IS NOT WORKING UNLESS APPROVED BY THE TRANSPORTATION DIVISION.
- FOR EXCAVATION PROTECTION AND SAFETY FENCE REQUIREMENTS SEE STANDARD 804S-4.
- THE USE OF ARROW DISPLAYS ARE REQUIRED ON ALL LANE CLOSURES. THE CONTRACTOR SHALL PROVIDE ONE (1) STAND-BY UNIT IN GOOD WORKING CONDITION AT THE JOB SITE, READY FOR USE IF THE OPERATION REQUIRES 24-HOUR A DAY LANE CLOSURE SET-UPS.

CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS
Kui Thurmond-Jones 4/4/11
ADOPTED

GENERAL NOTES

STANDARD NO. 804S-1
8 OF 9

THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.

Typical Transition Lengths and Suggested Maximum Spacing of Devices

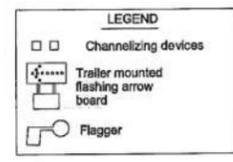
Speed KMPH	Posted Speed MPH	Formula	Minimum Desirable Taper Lengths (L) Meters (Feet)			Suggested Max. Device Spacing		"X" Dimension
			3.0(10) Offset Meters (feet)	3.3(11) Offset Meters (feet)	3.8(12) Offset Meters (feet)	On a taper Meters (feet)	On a tangent Meters (feet)	
50	30	L=WS ² /60	45 (150)	50 (165)	55 (180)	9 (30)	15-20 (60-75)	40 (120)
55	35		65 (205)	70 (225)	75 (245)	10 (35)	25-25 (70-90)	50 (160)
65	40		80 (265)	90 (295)	100 (320)	12 (40)	25-30 (80-100)	75 (240)
70	45	L=WS	135 (450)	150 (495)	165 (540)	13 (45)	25-30 (80-110)	100 (320)
80	50		180 (600)	195 (650)	200 (660)	15 (50)	30-35 (100-125)	120 (400)
90	55		195 (650)	200 (660)	205 (675)	16 (55)	35-40 (110-140)	150 (500)
95	60	L=WS	180 (600)	200 (660)	220 (720)	18 (60)	40-45 (120-150)	180 (600)
105	65		195 (650)	215 (715)	235 (780)	19 (65)	40-50 (130-165)	210 (700)
115	70		215 (700)	235 (770)	255 (840)	21 (70)	45-55 (140-175)	240 (800)

CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS
Kui Thurmond-Jones 4/4/11
ADOPTED

DEVICE SPACING

STANDARD NO. 804S-1
9 OF 9

THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.



REV. BY	DATE	REVISION DESCRIPTION

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
Matthew Baker
3/6/2024

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
AUSTIN STANDARD
ARTERIAL ONE LANE CLOSURE

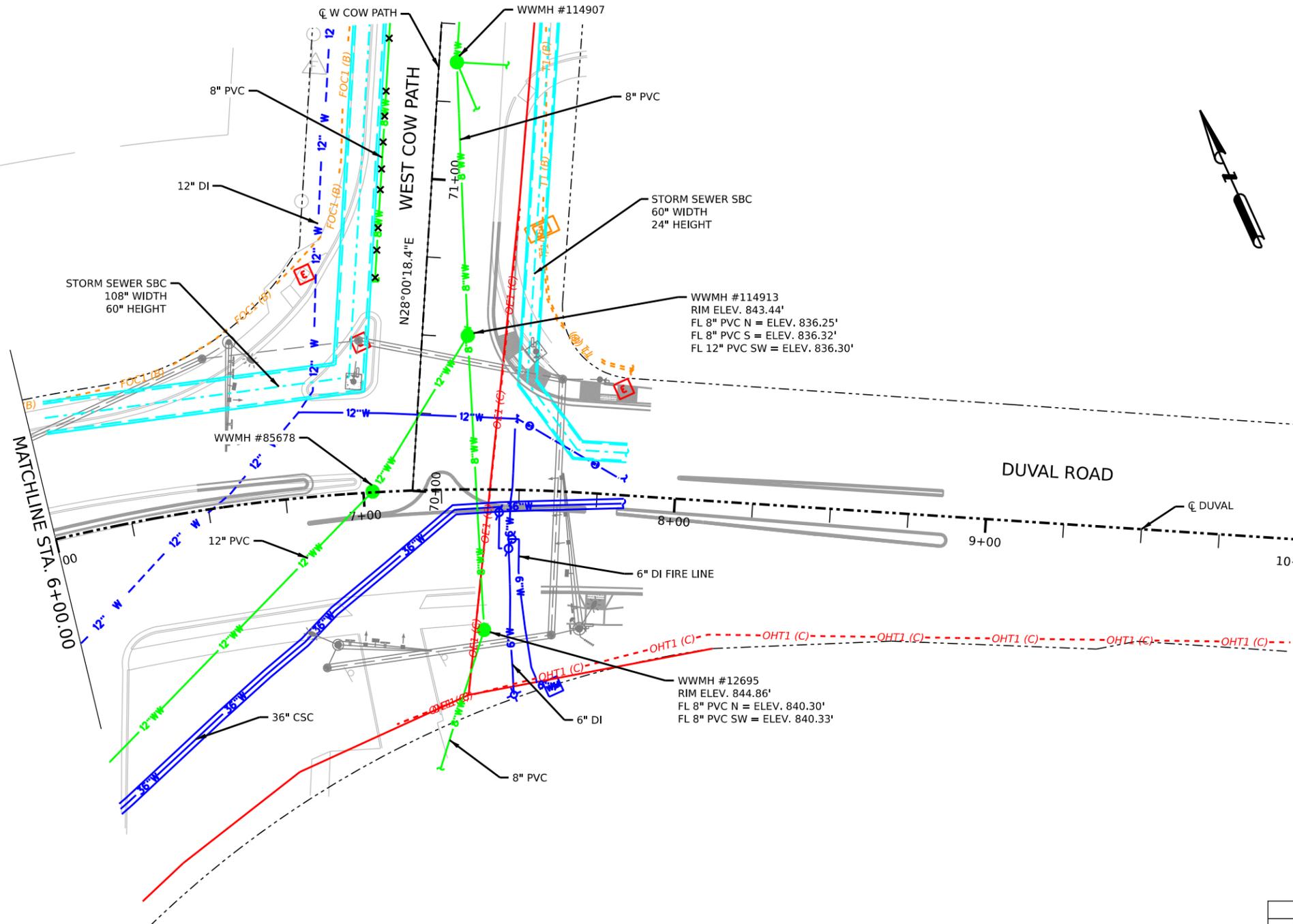
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BGE, Inc.
1701 Directors Boulevard, Suite 1000
Austin, TX 78744
Tel: 512-479-4400 • www.bgeinc.com
TBPE Registration No. F-1046

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FILE: G:\TXC\Projects\BOE Consulting\10941-00 Duval at W Cow Path\03_CADD\01_Shts\02-TCPI\Std\804S-1.dgn

EXISTING UTILITIES LEGEND

- 6" W 6" WATER CITY OF AUSTIN
- - - - 12" W 12" WATER CITY OF AUSTIN
- 10" W 10" WATER PRIVATE
- ==== 24" W 24" WATER CITY OF AUSTIN
- ==== 36" W 36" WATER CITY OF AUSTIN
- - - - W1 (D) WATER CITY OF AUSTIN
- - - - WW1 (B) WASTEWATER LINE
- - - - WW1 (D) WASTEWATER LINE
- - - - T1 (B) TELEPHONE ATT
- - - - FOC1 (B) FOC DUCT ATT
- - - - E1 (B) TRAFFIC SIGNAL
- - - - OE1 (C) OVHD ELEC POWER LINE
- - - - E1 (B) ELECTRIC AUSTIN ENERGY
- - - - OHT1 (C) OVHD TELEPHONE LINE
- OVHD ELECTRIC LINE
- - - - G1 (B) TEXAS GAS SERVICE
- x x x x ABANDONED UTILITY



4/9/2024

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION

DUVAL AT W COW PATH

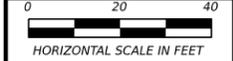
EXISTING UTILITIES - BASE MAP
STATION 6+00.00 TO 10+00.00



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AUSTIN UTILITY PROJECT #		
CROSSING LOCATION(S)	UTILITY	PROJECT #
5,7,14	5'x2' SBC STORM SEWER	W-1987-0003
2,12,13,15,18	9'x5' SBC STORM SEWER	UNKOWN
9,11	6" DI WATER	W-1976-0121
	6" DI FIRE LINE	W-1976-0121
1	12" DI WATER	W-1975-0262
7	12" UNK WATER	W-1976-0121
8	36" CSC WATER	W-1978-0038
	12" PVC WASTEWATER	S-1990-0016
3		S-1993-0009
	8" PVC WASTEWATER	S-1976-0053
4	8" PVC WASTEWATER	S-1993-0009
	8" PVC ABANDONED	UNKNOWN

NOTES	NAME	DATE
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REMOVAL LEGEND	
	REMOVE EXIST CURB/CURB AND GUTTER
	REMOVE EXIST CONC. MEDIAN
	REMOVE EXIST SIDEWALK
	REMOVE EXIST PVMT MARKING
	REMOVE EXIST CHANNELIZER POST

REV. NO.	DATE	REVISION DESCRIPTION

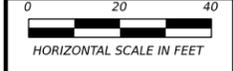
STATE OF TEXAS

 MATTHEW E. BAKER
 140924
 LICENSED PROFESSIONAL ENGINEER
Matthew Baker
 3/6/2024

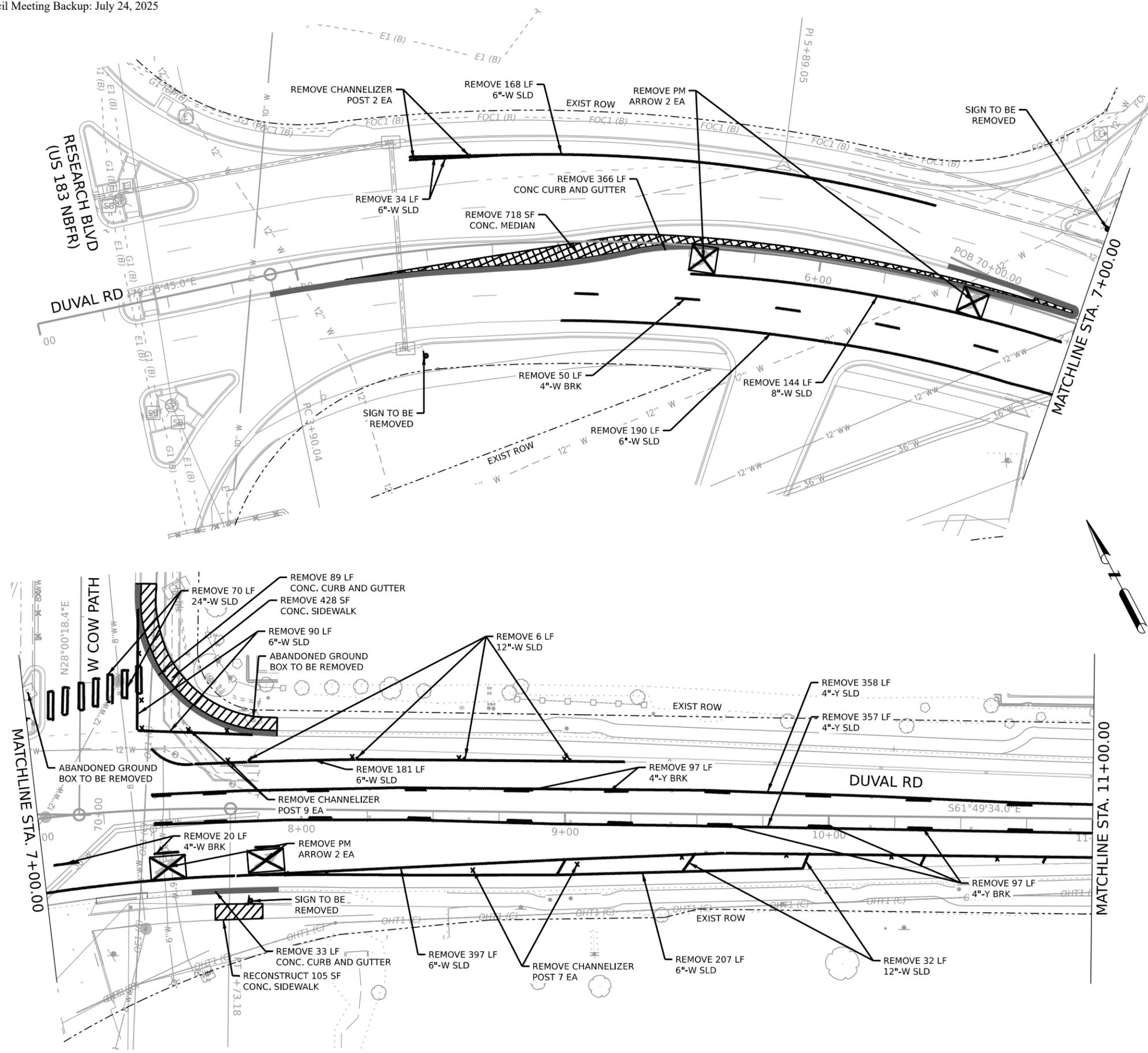
CITY OF AUSTIN, TEXAS
 AUSTIN TRANSPORTATION
 DUVAL AT W COW PATH
 INTERSECTION IMPROVEMENTS
 PROPOSED REMOVAL LAYOUT
 STA 3+00 TO STA 11+00

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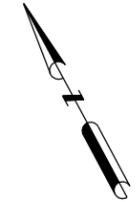
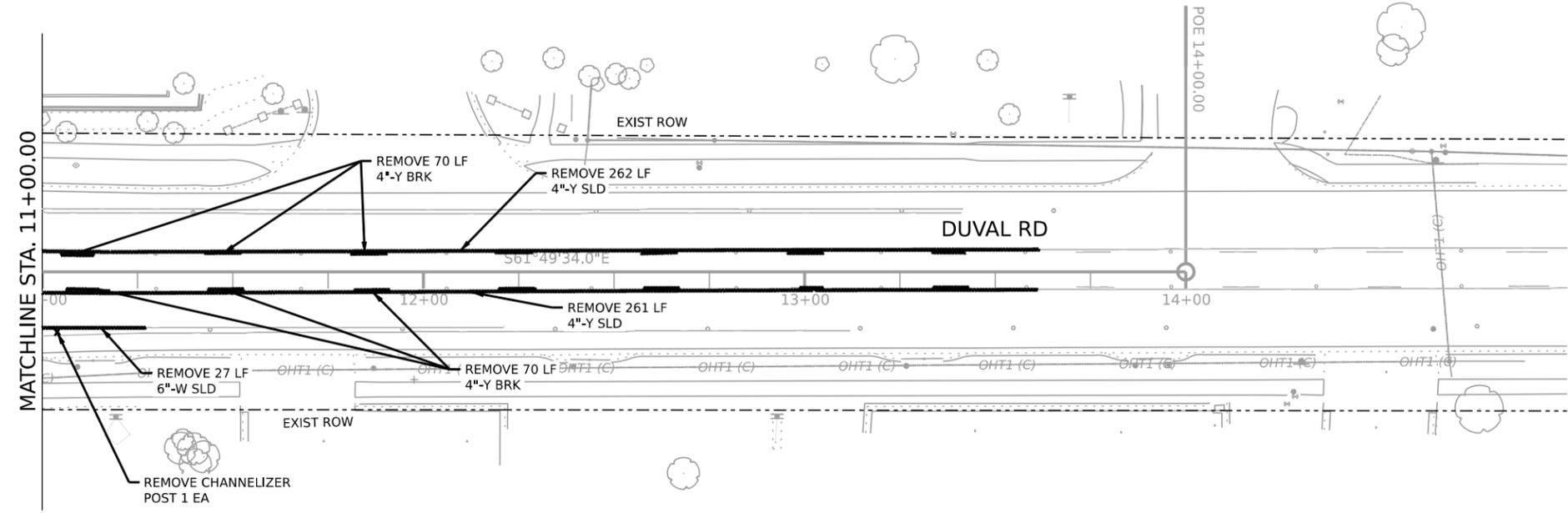
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REMOVAL LEGEND		File No.
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	REMOVE EXIST CONC. MEDIAN	
	REMOVE EXIST SIDEWALK	
	REMOVE EXIST PVMT MARKING	
	REMOVE EXIST CHANNELIZER POST	



STATE OF TEXAS

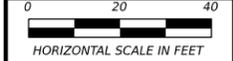
 MATTHEW E. BAKER
 140924
 LICENSED PROFESSIONAL ENGINEER
Matthew E. Baker
 3/6/2024

CITY OF AUSTIN, TEXAS
 AUSTIN TRANSPORTATION
 DUVAL AT W COW PATH
 INTERSECTION IMPROVEMENTS
 PROPOSED REMOVAL LAYOUT
 STA 11+00 TO STA 14+00

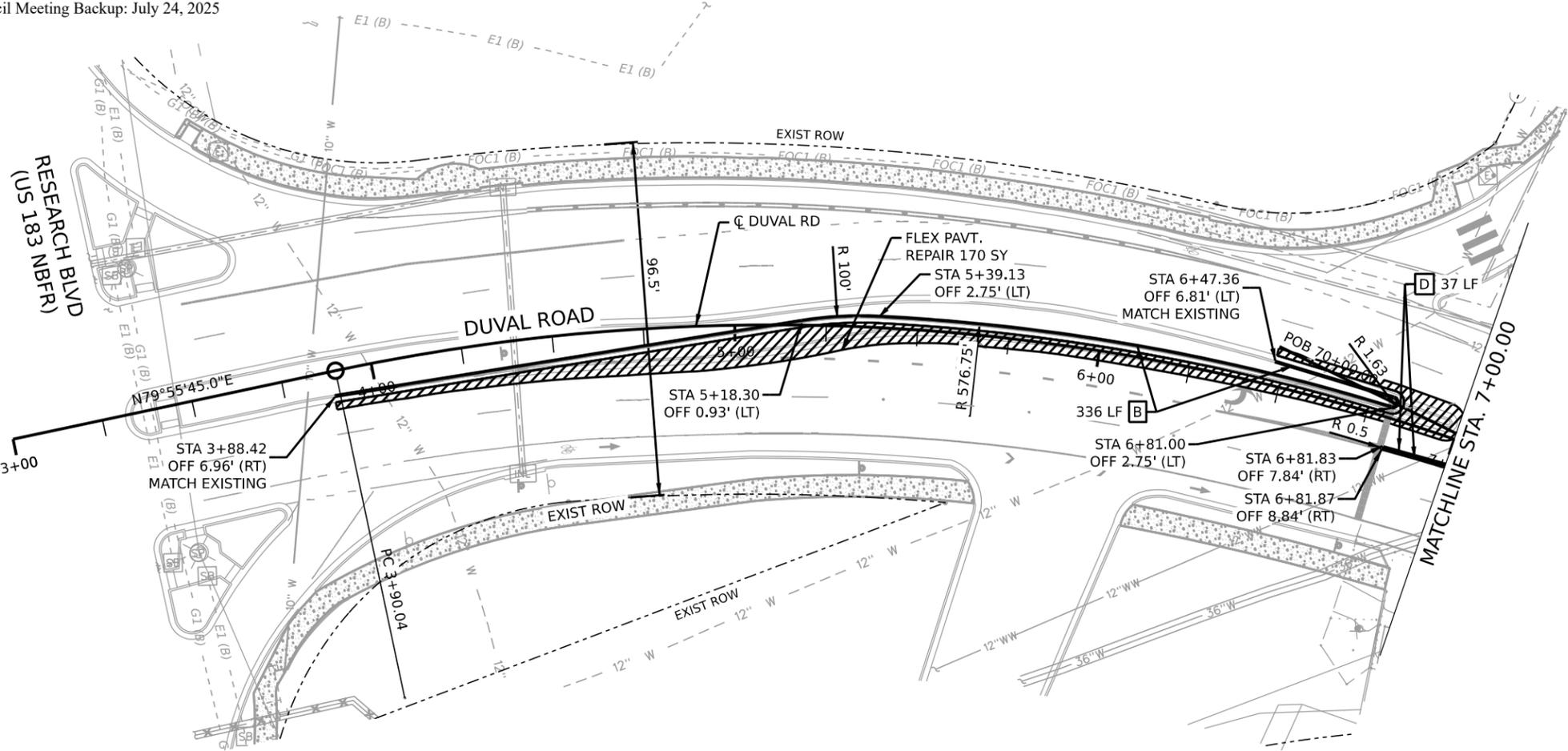


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ROADWAY LEGEND	
[A1]	P.C. SDWK CURB RAMP (TYPE I) (432S-RP-1) File 1
[A2]	P.C. SDWK CURB RAMP (COMB.) (SP432S-RP-2A)
[B]	P.C. CONCRETE CURB & GUTTER (430S-A)
[C]	P.C. CONCRETE SIDEWALK (6") (432S-6)
[D]	P.C. CONCRETE CURB (430S-C)
[E]	P.C. CONCRETE SIDEWALK (6") (432S-6)
[F]	DETECTABLE WARNING PAVERS (SS8000-SF)
[G]	EXISTING SIDEWALK TO REMAIN
[H]	CURB RAMP
[I]	PROP. FULL-DEPTH RECONSTRUCTION

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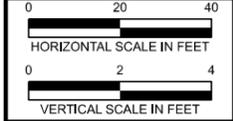
STATE OF TEXAS

 MATTHEW E. BAKER
 140924
 LICENSED
 PROFESSIONAL ENGINEER
 3/6/2024
 3/6/2024

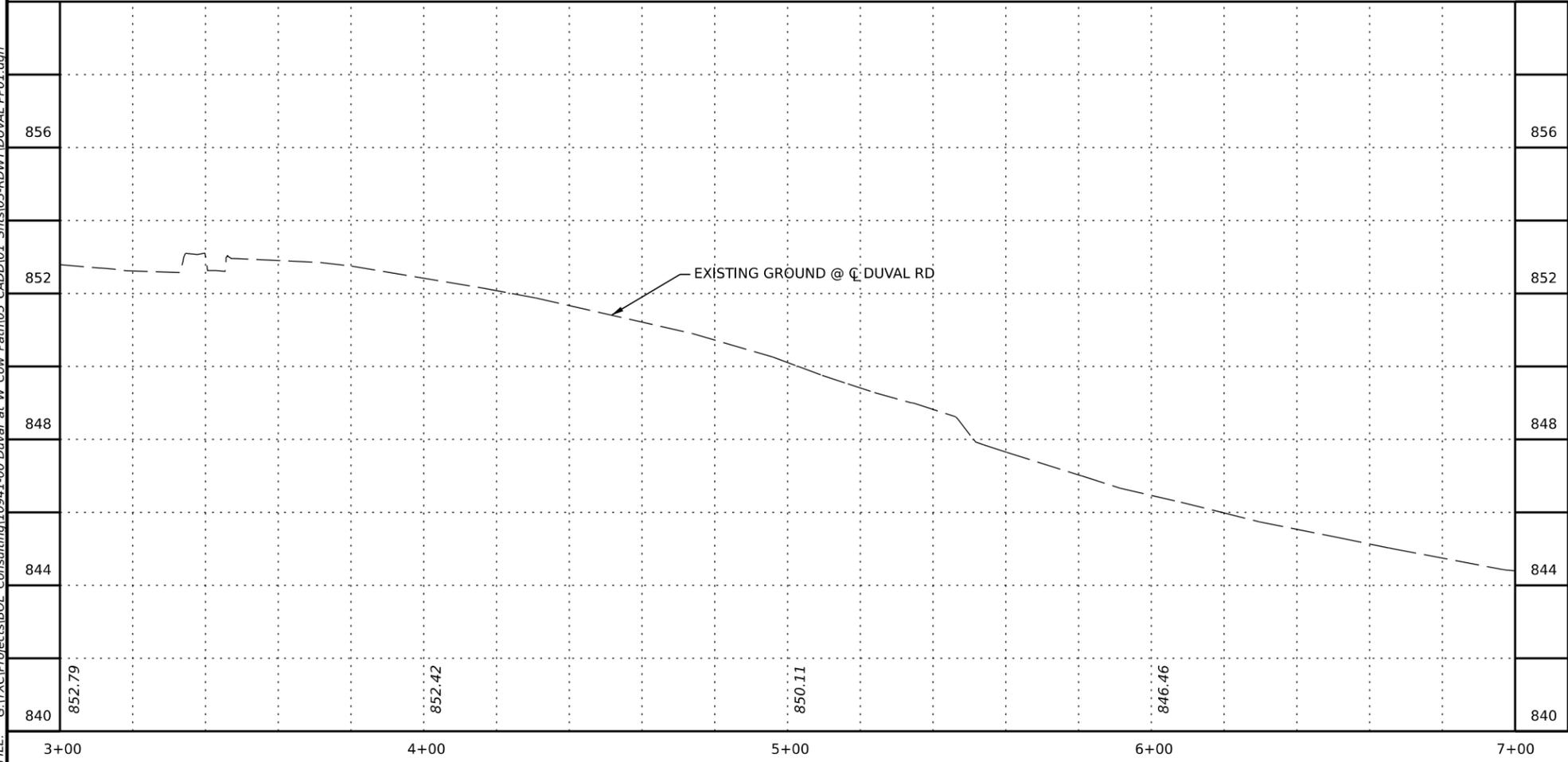
CITY OF AUSTIN, TEXAS
 AUSTIN TRANSPORTATION
 DUVAL AT W COW PATH
 INTERSECTION IMPROVEMENTS
 PROPOSED ROADWAY PLAN & PROFILE
 STA 3+00 TO STA 7+00

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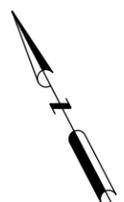
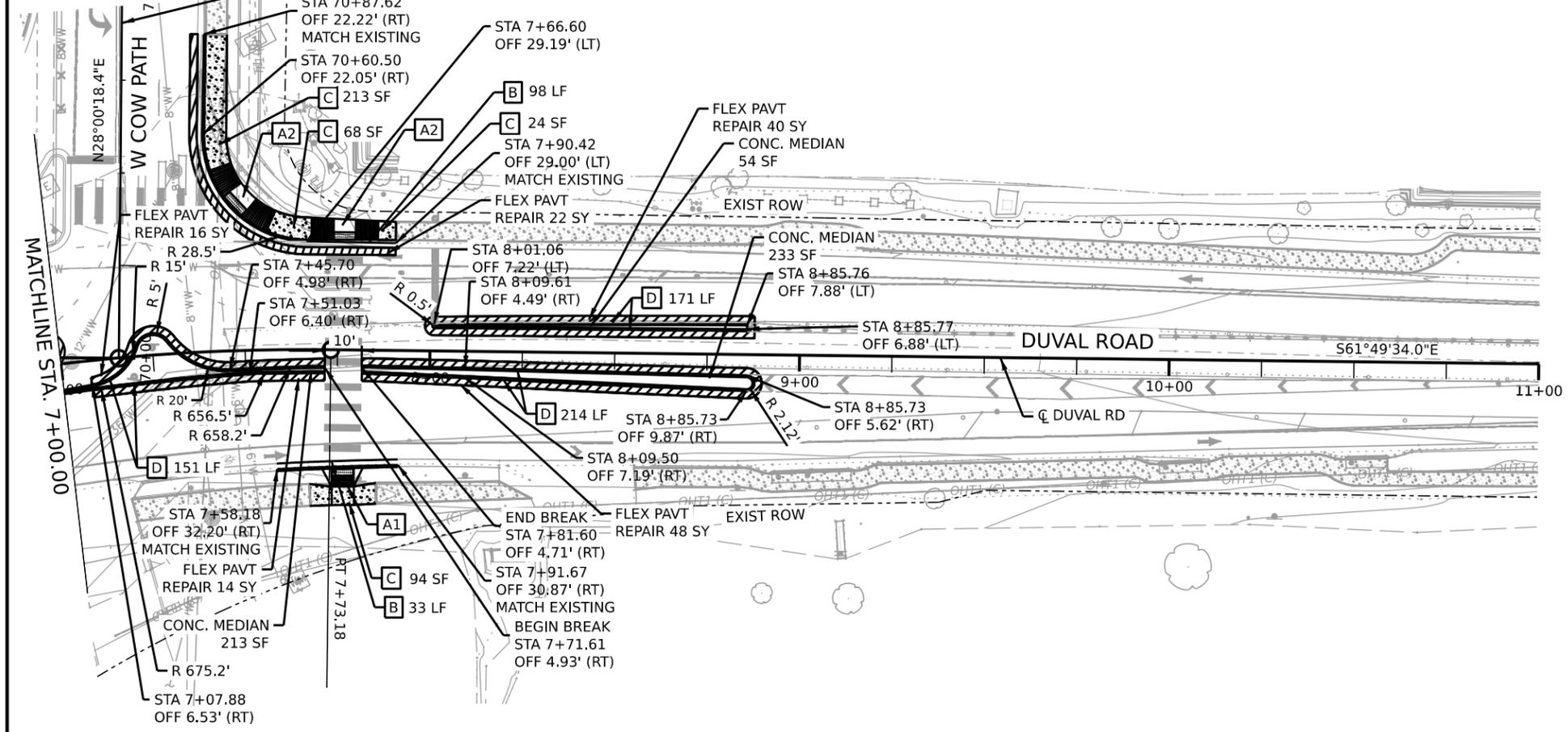
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DESIGNED BY	HK	
CHECKED BY	MB	
REVIEWED BY	SS	



- ALL DIMENSIONS AND ELEVATIONS ARE TO FOC UNLESS OTHERWISE NOTED.
- EXCAVATED MATERIAL SHALL BE USED FOR FILLING WHERE NECESSARY.
- THE CONTRACTOR SHALL ADJUST TO GRADE ALL PRIVATE UTILITY OWNER'S EXISTING STRUCTURES, VALVE BOXES, PULL BOXES, ETC. IN ACCORDANCE WITH THE APPLICABLE SPECIFICATIONS. THE CONTRACTOR SHALL COORDINATE THIS WORK WITH THE PRIVATE UTILITY OWNERS. SEE CONTACT INFORMATION IN GENERAL NOTES.
- CONTRACTOR TO SAWCUT 2' FROM PROPOSED/EXISTING CURB FLOWLINE/FACE OF CURB FOR FULL DEPTH RECONSTRUCTION.



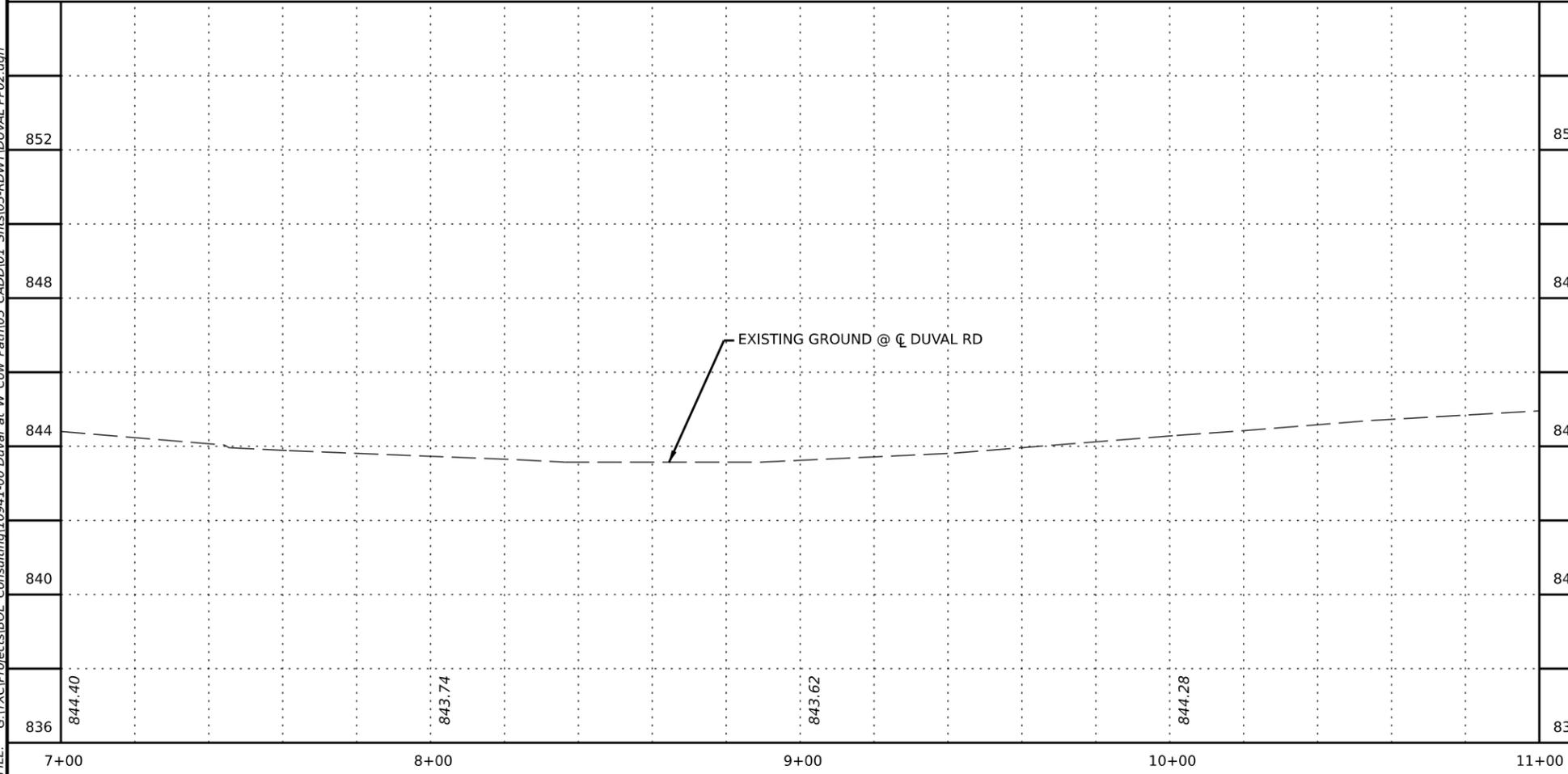
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ROADWAY LEGEND	
A1	P.C. SDWK CURB RAMP (TYPE I) (432S-RP-1) File 125-1303
A2	P.C. SDWK CURB RAMP (COMB.) (SP432S-RP-2A)
B	P.C. CONCRETE CURB & GUTTER (430S-A)
C	P.C. CONCRETE SIDEWALK (6") (432S-6)
D	P.C. CONCRETE CURB (430S-C)
	P.C. CONCRETE SIDEWALK (6") (432S-6)
	DETECTABLE WARNING PAVERS (SS8000-SF)
	EXISTING SIDEWALK TO REMAIN
	CURB RAMP
	PROP. FULL-DEPTH RECONSTRUCTION

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
3/6/2024

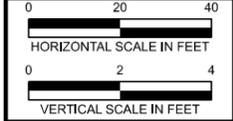
CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
PROPOSED ROADWAY PLAN & PROFILE
STA 7+00 TO STA 11+00



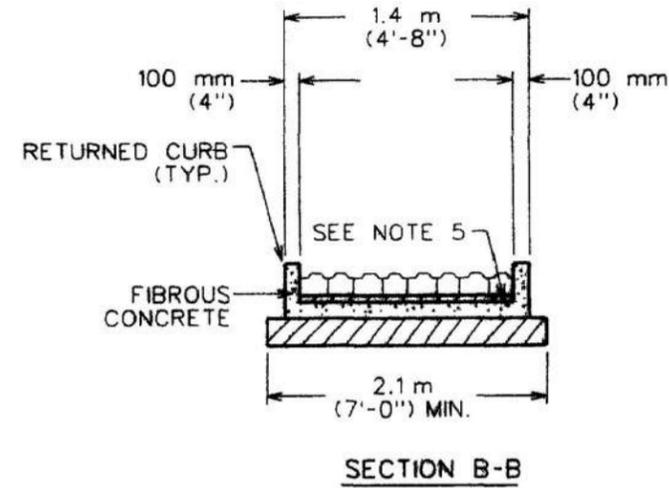
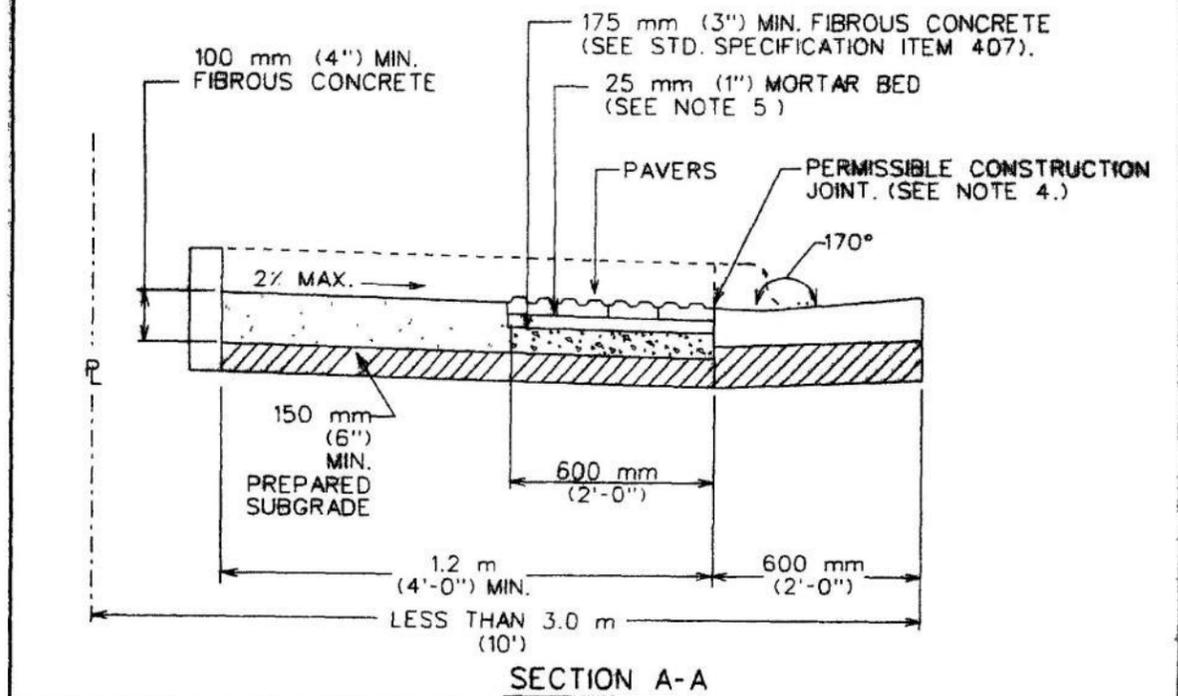
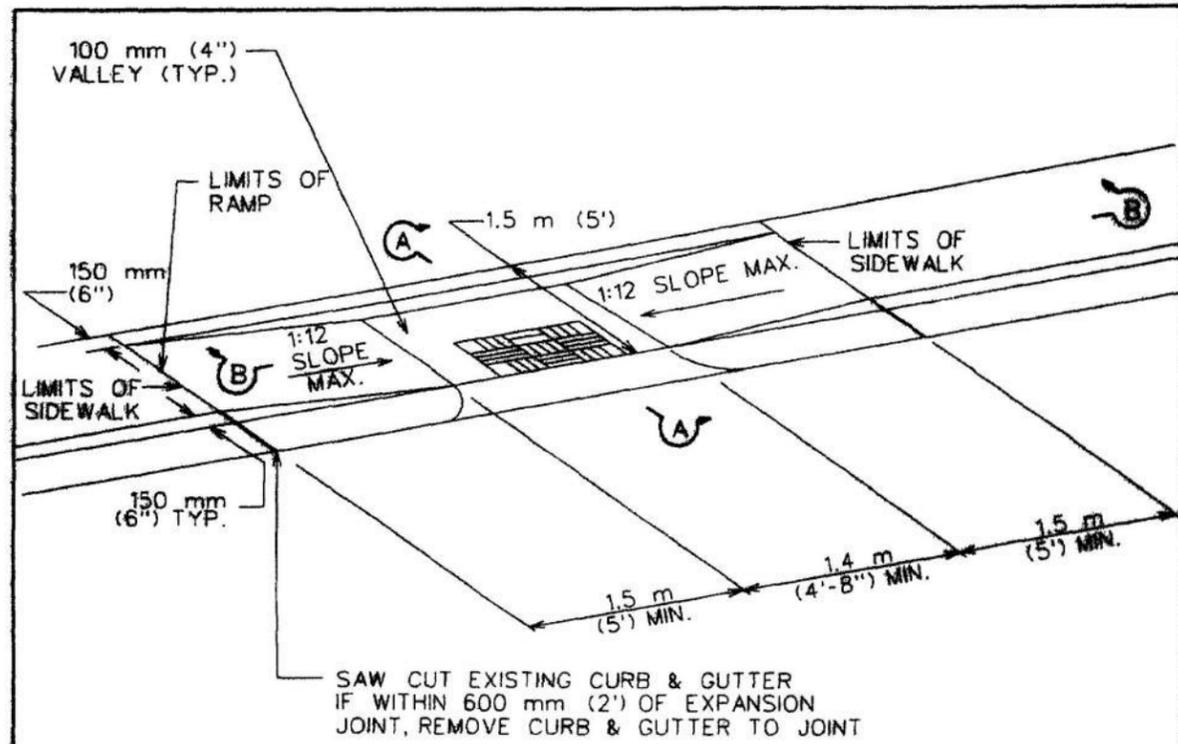
- ALL DIMENSIONS AND ELEVATIONS ARE TO FOC UNLESS OTHERWISE NOTED.
- EXCAVATED MATERIAL SHALL BE USED FOR FILLING WHERE NECESSARY.
- THE CONTRACTOR SHALL ADJUST TO GRADE ALL PRIVATE UTILITY OWNER'S EXISTING STRUCTURES, VALVE BOXES, PULL BOXES, ETC. IN ACCORDANCE WITH THE APPLICABLE SPECIFICATIONS. THE CONTRACTOR SHALL COORDINATE THIS WORK WITH THE PRIVATE UTILITY OWNERS. SEE CONTACT INFORMATION IN GENERAL NOTES.
- CONTRACTOR TO SAWCUT 2' FROM PROPOSED/EXISTING CURB FLOWLINE/FACE OF CURB FOR FULL DEPTH RECONSTRUCTION.

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- GENERAL NOTES:**
1. THIS STANDARD IS APPLICABLE FOR RAMP CONSTRUCTION WITHIN RIGHT-OF-WAY OR EASEMENT ONLY.
 2. PAVERS WILL HAVE DETECTABLE WARNING THAT CONSISTS OF RAISED TRUNCATED DOMES WITH A DIAMETER OF 23 mm (0.9"), A NOMINAL HEIGHT OF 5 mm (0.2") AND A NOMINAL CENTER TO CENTER SPACING OF 60 mm (2.35") AND SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT (Re: ADAAG SECTION 4.29.2). MATERIAL USED TO PROVIDE CONTRAST SHALL BE AN INTEGRAL PART OF THE WALKING SURFACE. A BASKET WEAVE PAVER PATTERN SHALL BE USED UNLESS DIRECTED BY THE ENGINEER OR DESIGNATED REPRESENTATIVE. PAVER PATTERN VARIES PER MANUFACTURER'S RECOMMENDATIONS. REFER TO STANDARD DETAIL 432S-5 FOR PAVER BASE PREPARATION DETAILS.
 3. THE CURB, GUTTER AND RAMP SYSTEM SHALL BE CONFIGURED TO MAINTAIN ALL RUNOFF FROM A 25 YEAR FREQUENCY STORM WITHIN THE RIGHT-OF-WAY (DRAINAGE CRITERIA MANUAL SECTION 1.2.2.B).
 4. TYPICAL SIDEWALK WIDTHS AND CURB RADII ARE SHOWN FOR ILLUSTRATION ONLY. REFER TO THE TRANSPORTATION CRITERIA MANUAL FOR SIDEWALK WIDTHS, CURB RADII AND CURB BASIC REQUIREMENTS.
 5. THE PERMISSIBLE CONSTRUCTION JOINT BETWEEN THE PAVERS AND THE ADJOINING SURFACE SHALL BE LIMITED TO 6 mm (1/4") JOINT SIZE. GAPS LARGER THAN 6 mm (1/4") MUST BE APPROVED BY THE ENGINEER OR DESIGNATED REPRESENTATIVE. ALL JOINTS BETWEEN BRICKS AND ADJOINING SURFACE SHALL BE MORTAR FILLED UNLESS DIRECTED OTHERWISE BY THE ENGINEER OR DESIGNATED REPRESENTATIVE.
 6. MORTAR SHALL CONFORM TO STD. SPECIFICATION ITEM SECTION 403S.3.5, MORTAR AND GROUT. ALL OTHER CONCRETE SHALL CONFORM TO STD. SPECIFICATION ITEM 403S, CONCRETE FOR STRUCTURES, UNLESS OTHERWISE NOTED.
 7. CURB RAMPS WITH 100 mm (4") CONCRETE VALLEY MAY ONLY BE USED WHERE PEDESTRIANS WOULD NOT NORMALLY WALK DIAGONALLY ACROSS THE RAMP.

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS	COMBINED SIDEWALK CURB RAMP WITH PAVERS WITHIN LIMITED ROW ONLY
<i>Bill Gardner</i> 4/21/07 ADOPTED	STANDARD NO. 432S-3G 1 OF 2
THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS	COMBINED SIDEWALK CURB RAMP WITH PAVERS WITHIN LIMITED ROW ONLY
<i>Bill Gardner</i> 4/21/07 ADOPTED	STANDARD NO. 432S-3G 2 OF 2
THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	

REV. NO.	DATE	REVISION DESCRIPTION

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
Matthew E. Baker
3/6/2024
3/6/2024

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
AUSTIN STANDARD
COMBINED SIDEWALK CURB RAMP WITH PAVERS

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MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
3/6/2024
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CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
SMD(GEN)-08: SGN MOUNT DETAIL SM.
ROADSIDE SGN GEN NOTES & DETAIL

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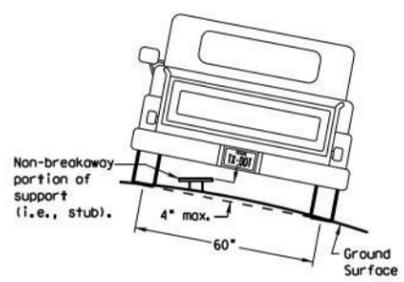
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SIGN SUPPORT DESCRIPTIVE CODES
(Descriptive Codes correspond to project estimate and quantities sheets)

SM RD SGN ASSM TY XXXXX(X)XX(X-XXXX)

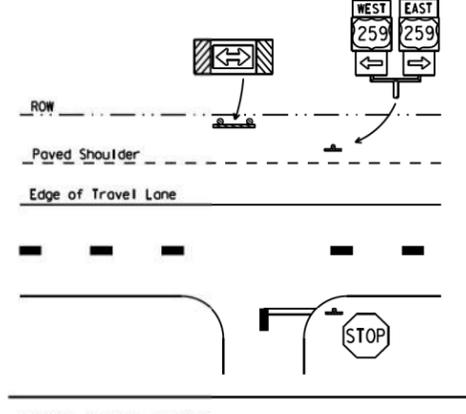
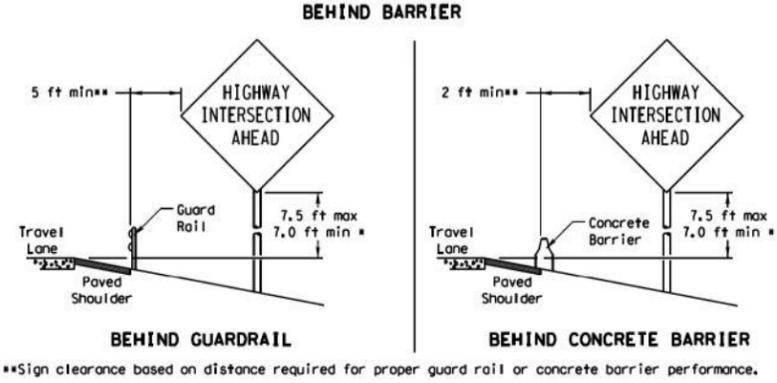
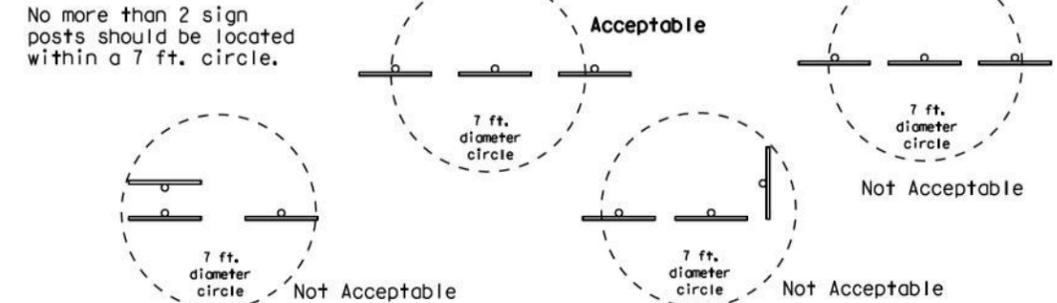
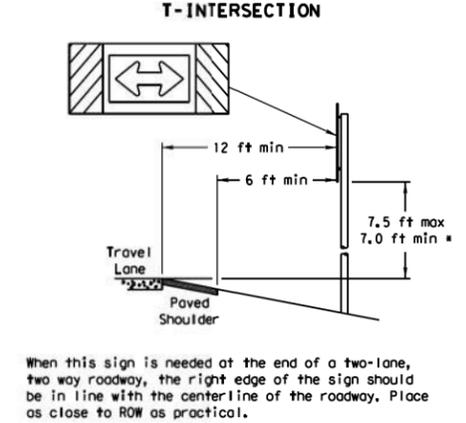
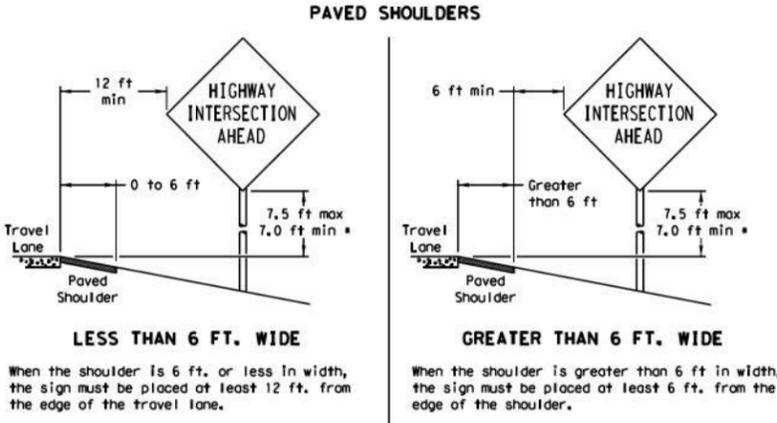
- Post Type**
FRP = Fiberglass Reinforced Plastic Pipe (see SMD(FRP))
TWT = Thin-Walled Tubing (see SMD(TWT))
10BWG = 10 BWG Tubing (see SMD(SLIP-1) to (SLIP-3))
S80 = Schedule 80 Pipe (see SMD(SLIP-1) to (SLIP-3))
- Number of Posts (1 or 2)**
- Anchor Type**
UA = Universal Anchor - Concreted (see SMD(FRP) and (TWT))
UB = Universal Anchor - Bolted down (see SMD(FRP) and (TWT))
WS = Wedge Anchor Steel - (see SMD(TWT))
WP = Wedge Anchor Plastic (see SMD(TWT))
SA = Slipbase - Concreted (see SMD(SLIP-1) to (SLIP-3))
SB = Slipbase - Bolted Down (see SMD(SLIP-1) to (SLIP-3))
- Sign Mounting Designation**
P = Prefab. "Plain" (see SMD(SLIP-1) to (SLIP-3), (TWT), (FRP))
T = Prefab. "T" (see SMD(SLIP-1) to (SLIP-3), (TWT))
U = Prefab. "U" (see SMD(SLIP-1) to (SLIP-3))
IF REQUIRED
1EXT or 2EXT = Number of Extensions (see SMD(SLIP-1) to (SLIP-3), (TWT))
BM = Extruded Wind Beam (see SMD(SLIP-1) to (SLIP-3))
WC = 1, 1.2 #/ft Wing Channel (see SMD(SLIP-1) to (SLIP-3))
EXAL = Extruded Aluminum Sign Panels (see SMD(SLIP-3))

REQUIRED CLEARANCE FOR BREAKAWAY SUPPORT



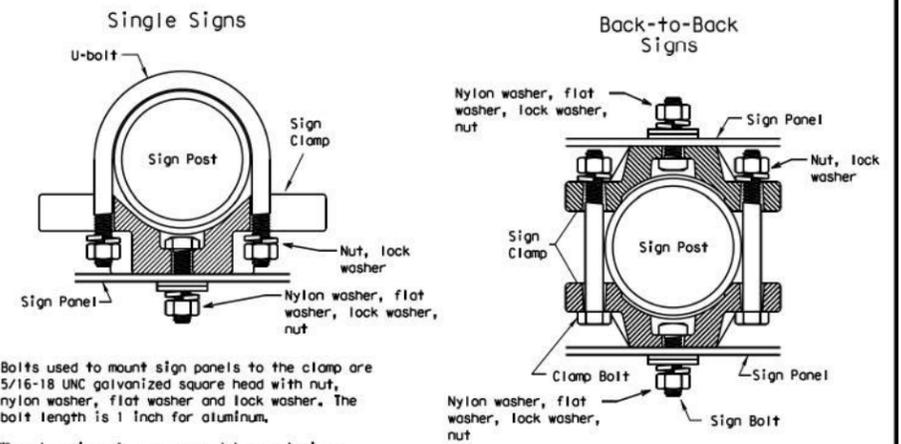
To avoid vehicle undercarriage snagging, any substantial remains of a breakaway support, when it is broken away, should not project more than 4 inches above a 60-inch chord (i.e., typical space between wheel paths).

SIGN LOCATION



- * Signs shall be mounted using the following condition that results in the greatest sign elevations
- (1) a minimum of 7 to a maximum of 7.5 feet above the edge of the travel lane or
 - (2) a minimum of 7 to a maximum of 7.5 feet above the grade at the base of the support when sign is installed on the backslope.
- The maximum values may be increased when directed by the Engineer.
- See the Traffic Operations Division website for detailed drawings of sign clamps, Triangular Slipbase System components and Wedge Anchor System components.
- The website address is:
<http://www.txdot.gov/publications/traffic.htm>

TYPICAL SIGN ATTACHMENT DETAIL



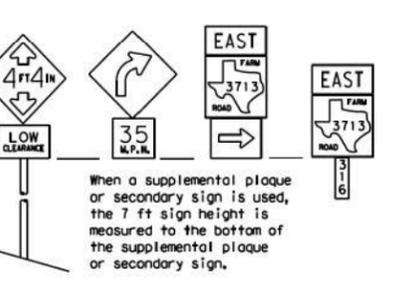
Bolts used to mount sign panels to the clamp are 5/16-18 UNC galvanized square head with nut, nylon washer, flat washer and lock washer. The bolt length is 1 inch for aluminum.

When two sign clamps are used to mount signs back-to-back, use a 5/16-18 UNC galvanized hex head per ASTM A307 with nut and helical-spring lock washer. The approximate bolt lengths for various post sizes and sign clamp types are given in the table at right. The bolt length may need to be adjusted depending upon field conditions.

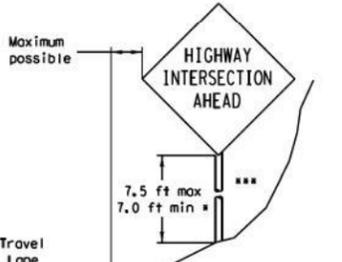
Sign clamps may be either the specific size clamp or the universal clamp.

Pipe Diameter	Approximate Bolt Length	
	Specific Clamp	Universal Clamp
2" nominal	3"	3 or 3 1/2"
2 1/2" nominal	3 or 3 1/2"	3 1/2 or 4"
3" nominal	3 1/2 or 4"	4 1/2"

SIGNS WITH PLAQUES



RESTRICTED RIGHT-OF-WAY (When 6 ft min. is not possible.)

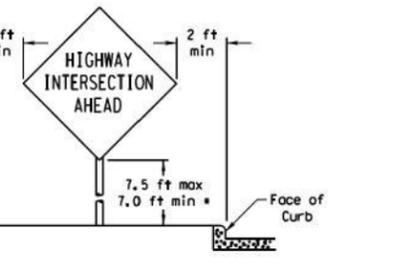


Right-of-way restrictions may be created by rocks, water, vegetation, forest, buildings, a narrow island, or other factors.

In situations where a lateral restriction prevents the minimum horizontal clearance from the edge of the travel lane, signs should be placed as far from the travel lane as practical.

*** Post may be shorter if protected by guardrail or if Engineer determines the post could not be hit due to extreme slope.

CURB & GUTTER OR RAISED ISLAND



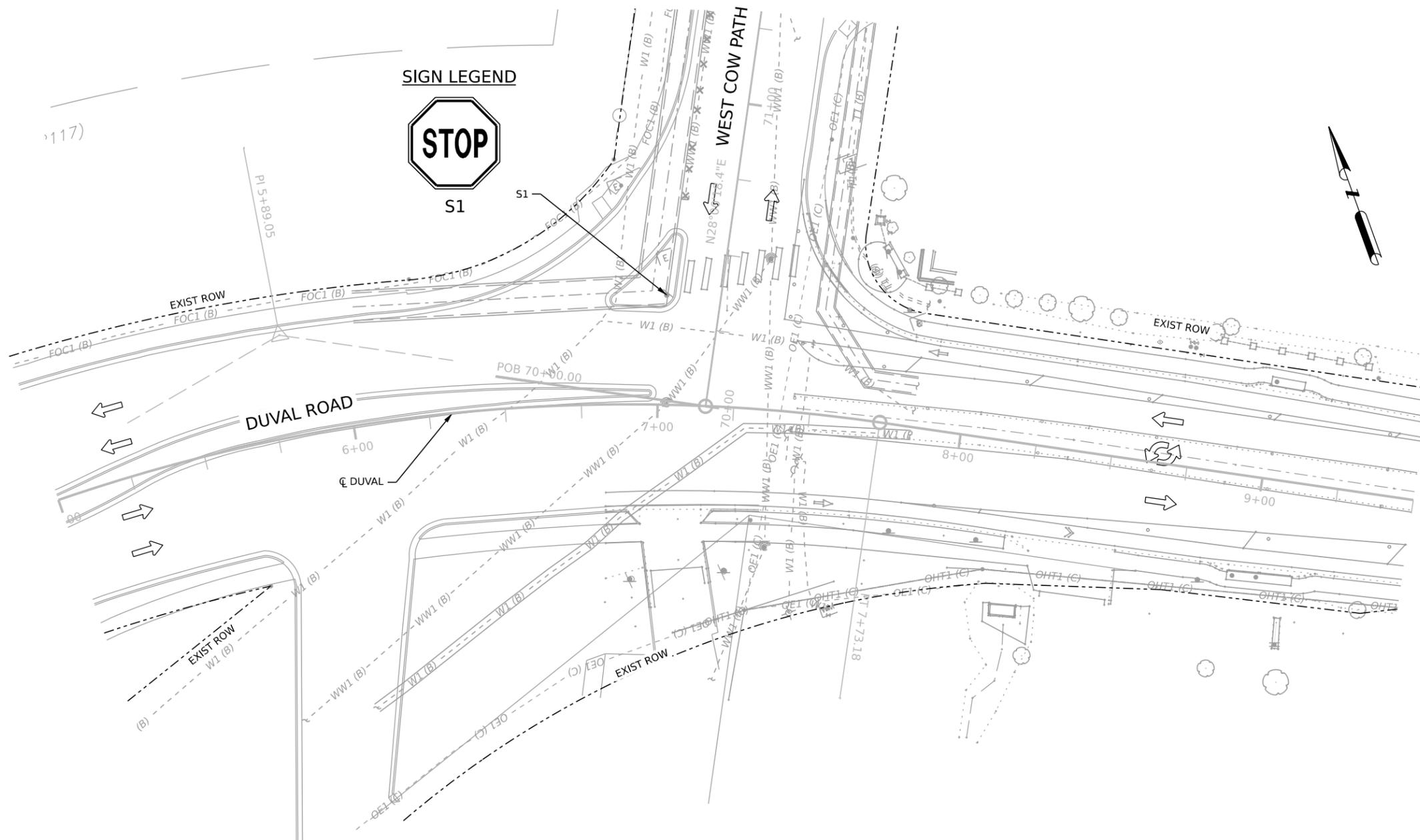
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EXIST SIGNALS LEGEND		REV	DATE	DESCRIPTION
	TRAVEL DIRECTION ARROW			
	EXIST. TRAFFIC POLE			
	EXIST. POLE FOUNDATION			
	EXIST. TRAFFIC SIGNAL HEAD			
	SIGNAL HEADS NUMBER			
	EXIST. PEDESTAL POLE			
	EXIST. PEDESTRIAN PUSH BUTTON			
	EXIST. PEDESTRIAN SIGNAL HEAD			
	EXIST. LUMINAIRE			
	EXIST. RADAR DETECTION			

SIGN LEGEND



S1



STATE OF TEXAS

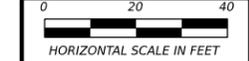
 MATTHEW E. BAKER
 140924
 LICENSED PROFESSIONAL ENGINEER
Matthew E. Baker
 3/6/2024

CITY OF AUSTIN, TEXAS
 AUSTIN TRANSPORTATION
 DUVAL AT W COW PATH
 INTERSECTION IMPROVEMENTS
 DUVAL AT W COW PATH
 EXIST SIGNAL LAYOUT



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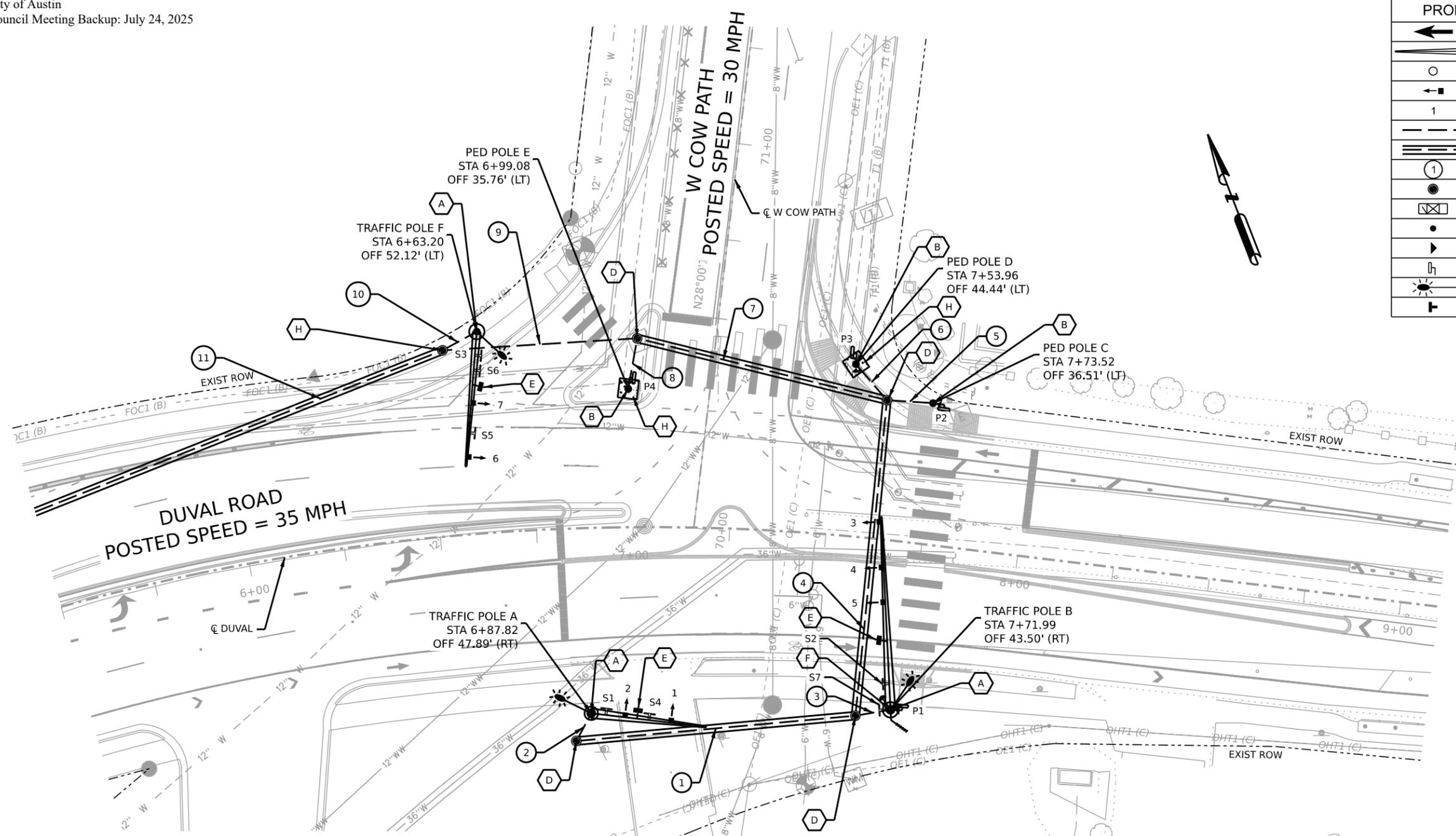


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PROPOSED SIGNALS LEGEND	
	TRAVEL DIRECTION ARROW
	PROP. TRAFFIC POLE
	PROP. POLE FOUNDATION
	PROP. TRAFFIC SIGNAL HEAD
	SIGNAL HEADS NUMBER
	PROP. CONDUIT (TRENCH)
	PROP. CONDUIT (BORE)
	CONDUIT NUMBER
	PROP. PULL BOX
	PROP. CONTROLLER
	PROP. PEDESTAL POLE
	PROP. PEDESTRIAN PUSH BUTTON
	PROP. PEDESTRIAN SIGNAL HEAD
	PROP. LUMINAIRE
	PROP. RADAR DETECTION

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3/6/2024
3/6/2024



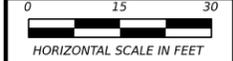
- NOTES:
1. THE STATION AND OFFSET ARE MEASURED FROM THE CENTERLINE OF DUVAL RD. TO CENTER OF POLES. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD AND VERIFIED WITH CITY OF AUSTIN PRIOR TO ANY CONSTRUCTION.
 2. THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN APPROXIMATE WAY ONLY. CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THEIR FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
 3. ALL VEHICULAR TRAFFIC SIGNAL HEAD TYPES HAVE 12-INCH LENSES.
 4. SIDEWALK AND RAMP SHALL CONFORM TO ADA AND TDLR STANDARDS. RUNNING SLOPE ALONG SIDEWALK SHALL NOT EXCEED 5% AND CROSS SLOPE SHALL NOT EXCEED 1.5%. RAMP SHALL NOT EXCEED 1:12 SLOPE.
 5. ALL PED PUSH BUTTONS SHALL BE ACCESSIBLE FROM A LEVEL LANDING AREA. LOCATION OF PUSH BUTTONS SHALL BE COMPLIANT WITH TXMUTCD SECTION 4E RECOMMENDED LOCATIONS.
 6. SIDEWALK SHALL BE EXTENDED UP TO SIGNAL POLES AS NEEDED TO PROVIDE ACCESS TO PEDESTRIAN PUSH BUTTONS.
 7. CONTRACTOR SHALL POTHOLE LOCATIONS SHOWN BEFORE FOUNDATION INSTALLATION. SHOULD THE CONTRACTOR VISUALLY CONFIRM THE PRESENCE OF ROCK WITHIN SIX (6) FEET OF THE SURFACE, CONTRACTOR SHALL NOTIFY THE CITY OF AUSTIN TRAFFIC SIGNAL INSPECTOR TO EVALUATE THE POSSIBILITY OF UTILIZING A DIFFERENT FOUNDATION SPECIFICATION GIVEN THE PRESENCE OF ROCK WITHIN THE SUBGRADE. IF THE CITY OF AUSTIN TRAFFIC SIGNAL INSPECTOR DEEMS THAT THE PRESENCE OF ROCK IS ADEQUATE FOR THE USE OF A DIFFERENT FOUNDATION SPECIFICATION, CONTRACTOR SHALL NOTIFY THE ENGINEER OF RECORD OF THE CHANGE PRIOR TO PROCEEDING.
 8. ELECTRICAL SERVICE TO BE DETERMINED BY AUSTIN ENERGY.

- INSTALLATION NOTES
- A** INSTALL SIGNAL POLE AND FOUNDATION AS SHOWN IN THE PLANS AND SUMMARIZED UNDER POLE SCHEDULE. SEE COA SIGNAL FOUNDATION STANDARD FOR FOUNDATION DETAILS AND DETAILS FOR TRAFFIC SIGNAL MAST ARM DETAILS.
 - B** INSTALL PEDESTAL POLE AND FOUNDATION AS SHOWN IN PLANS AND SUMMARIZED UNDER POLE SCHEDULE. SEE COA STANDARD 831S-2 FOR FOUNDATION DETAILS.
 - D** INSTALL TYPE B GROUND BOX. SEE COA STANDARDS 834S-3 AND 834S-4.
 - E** INSTALL RADAR AS SHOWN IN THE PLANS.
 - F** INSTALL HANWHA QNP-6250RH CCTV CAMERA.
 - H** INSTALL TYPE C GROUND BOX. SEE COA STANDARDS 834S-5 AND 834S-6.
 - I** INSTALL PEDESTAL POLE AND SLAB FOUNDATION AS SHOWN IN PLANS AND SUMMARIZED IN POLE SCHEDULE. SEE FOUNDATION DETAIL FOR PEDESTAL POLE WITH PUSH BUTTON ONLY.
 - #** INSTALL CONDUITS AS SHOWN IN THE PLANS AND SUMMARIZED UNDER THE CONDUIT AND CONDUCTOR SCHEDULE.

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
DUVAL AT W COW PATH
PROPOSED SIGNAL LAYOUT



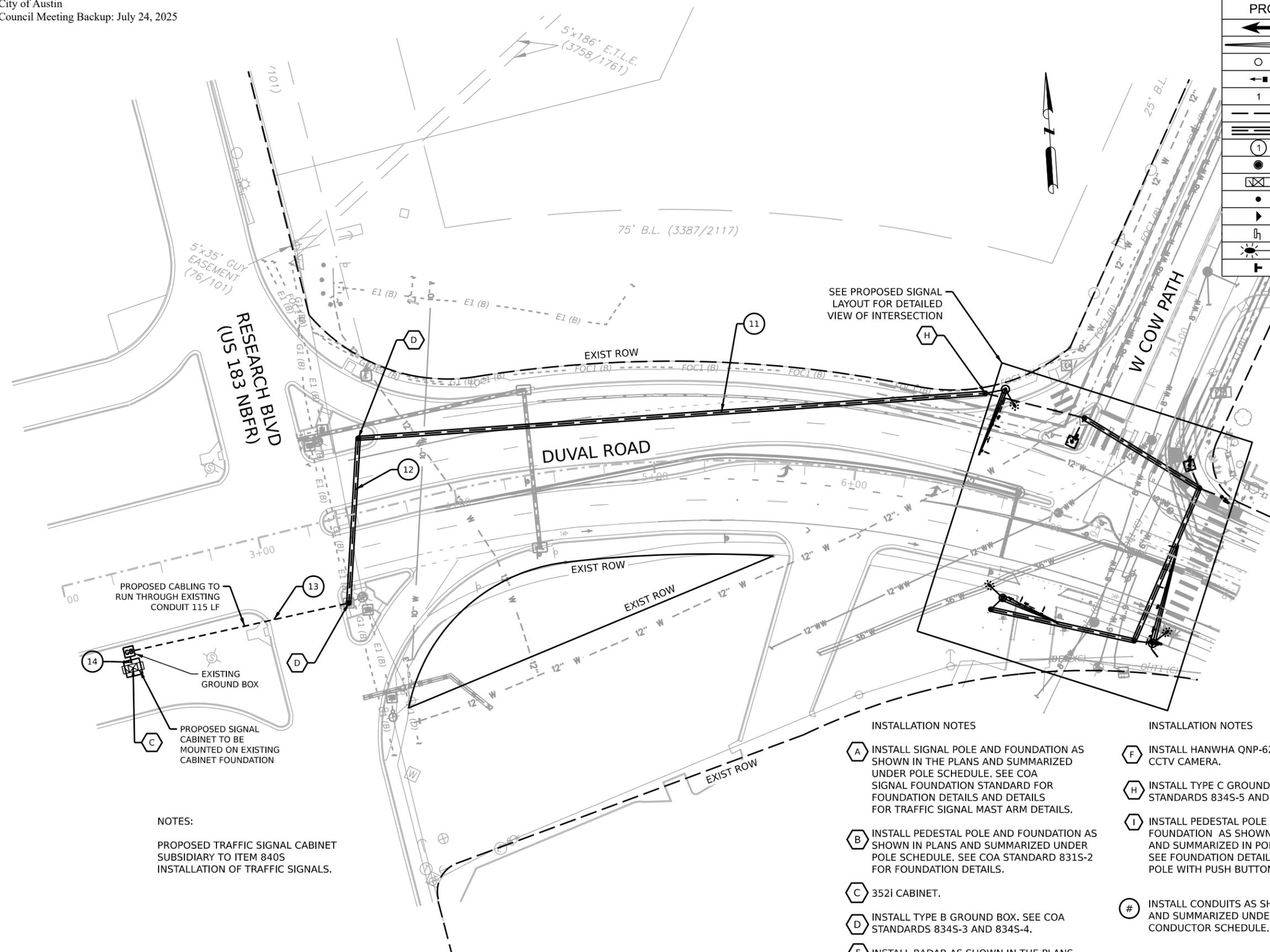
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PROPOSED SIGNALS LEGEND	
	TRAVEL DIRECTION ARROW
	PROP. TRAFFIC POLE
	PROP. POLE FOUNDATION
	PROP. TRAFFIC SIGNAL HEAD
	SIGNAL HEADS NUMBER
	PROP. CONDUIT (TRENCH)
	PROP. CONDUIT (BORE)
	CONDUIT NUMBER
	PROP. PULL BOX
	PROP. CONTROLLER
	PROP. PEDESTAL POLE
	PROP. PEDESTRIAN PUSH BUTTON
	PROP. PEDESTRIAN SIGNAL HEAD
	PROP. LUMINAIRE
	PROP. RADAR DETECTION

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
3/6/2024
3/6/2024



NOTES:
PROPOSED TRAFFIC SIGNAL CABINET SUBSIDIARY TO ITEM 840S INSTALLATION OF TRAFFIC SIGNALS.

INSTALLATION NOTES

- A INSTALL SIGNAL POLE AND FOUNDATION AS SHOWN IN THE PLANS AND SUMMARIZED UNDER POLE SCHEDULE. SEE COA SIGNAL FOUNDATION STANDARD FOR FOUNDATION DETAILS AND DETAILS FOR TRAFFIC SIGNAL MAST ARM DETAILS.
- B INSTALL PEDESTAL POLE AND FOUNDATION AS SHOWN IN PLANS AND SUMMARIZED UNDER POLE SCHEDULE. SEE COA STANDARD 831S-2 FOR FOUNDATION DETAILS.
- C 352I CABINET.
- D INSTALL TYPE B GROUND BOX. SEE COA STANDARDS 834S-3 AND 834S-4.
- E INSTALL RADAR AS SHOWN IN THE PLANS.

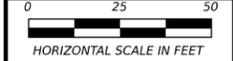
INSTALLATION NOTES

- F INSTALL HANWHA QNP-6250RH CCTV CAMERA.
- H INSTALL TYPE C GROUND BOX. SEE COA STANDARDS 834S-5 AND 834S-6.
- I INSTALL PEDESTAL POLE AND SLAB FOUNDATION AS SHOWN IN PLANS AND SUMMARIZED IN POLE SCHEDULE. SEE FOUNDATION DETAIL FOR PEDESTAL POLE WITH PUSH BUTTON ONLY.
- # INSTALL CONDUITS AS SHOWN IN THE PLANS AND SUMMARIZED UNDER THE CONDUIT AND CONDUCTOR SCHEDULE.

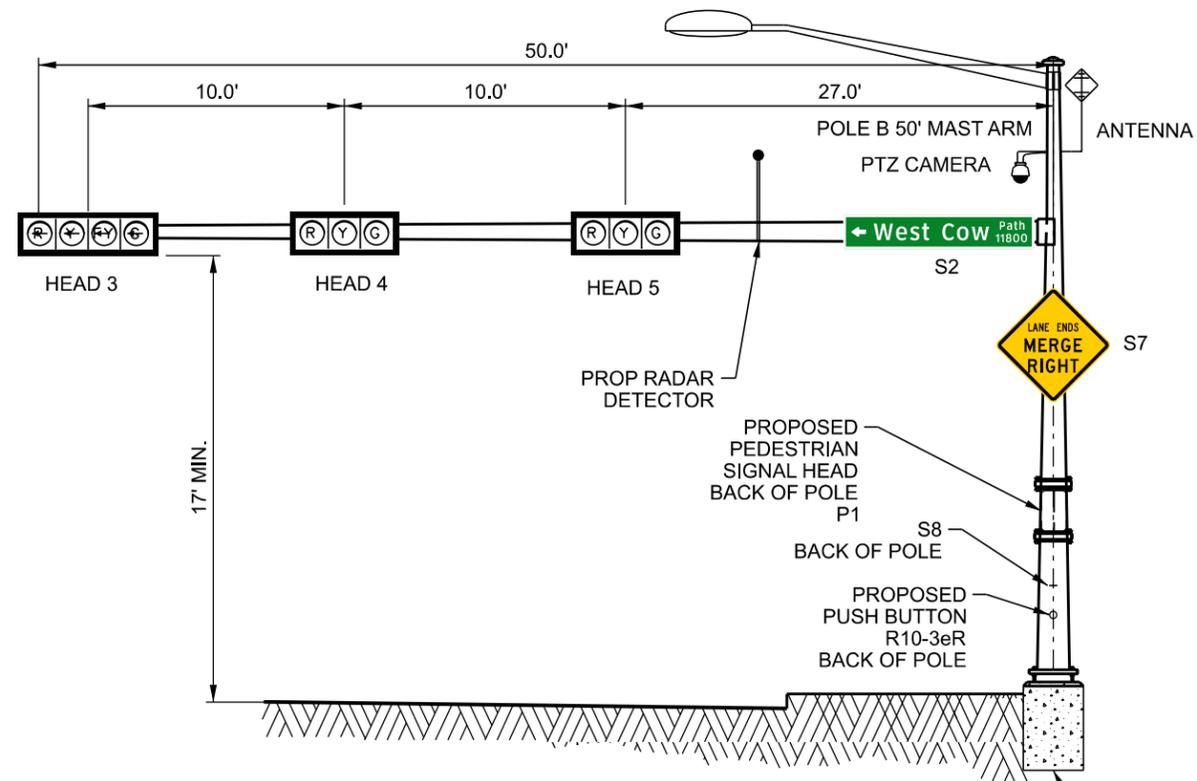
CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
CONDUIT LAYOUT

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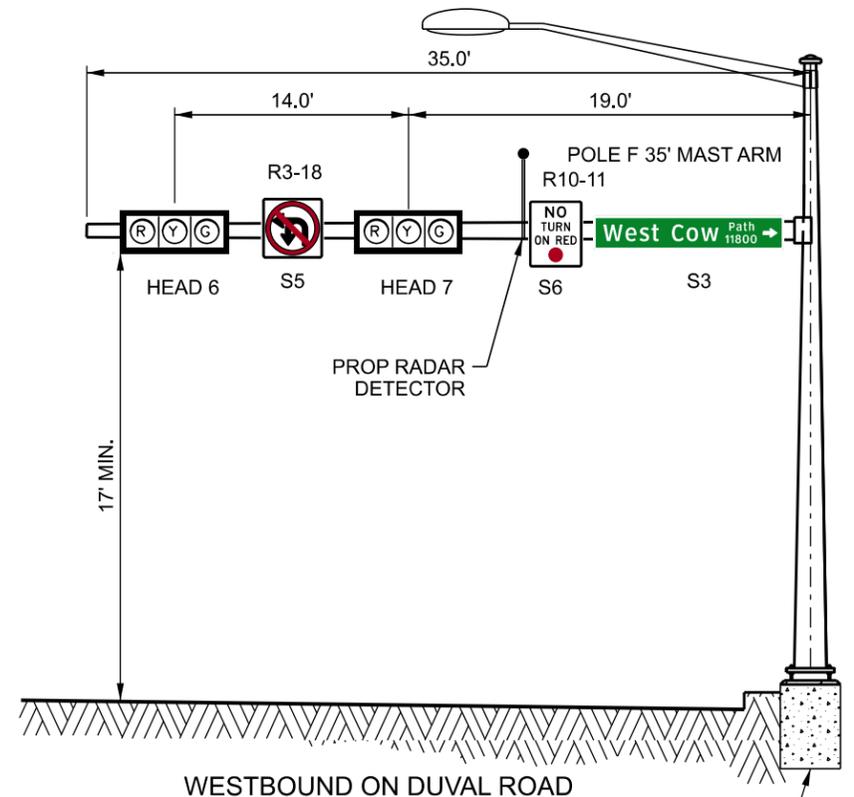
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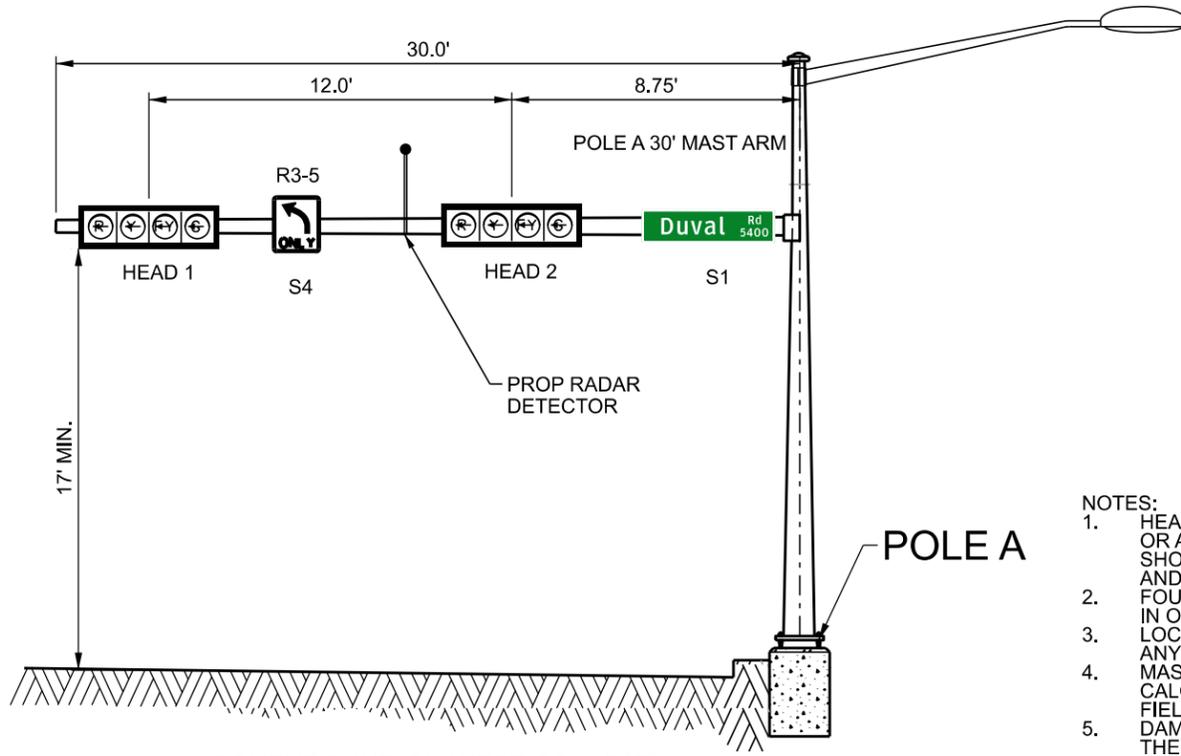
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EASTBOUND ON DUVAL ROAD
N.T.S.
PROPOSED POLE, SIGNALS AND SIGNS
POLE B

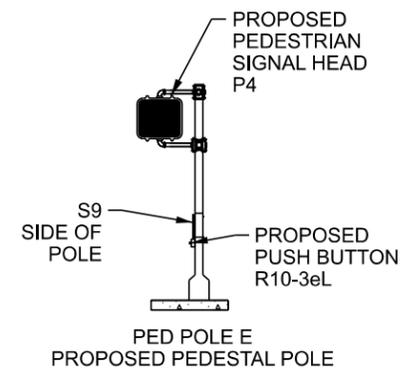
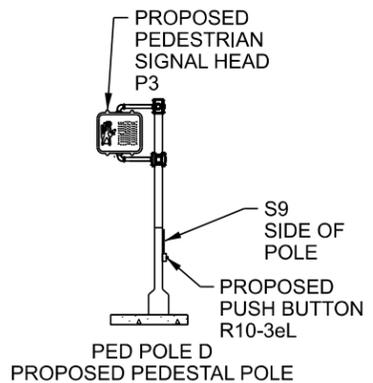
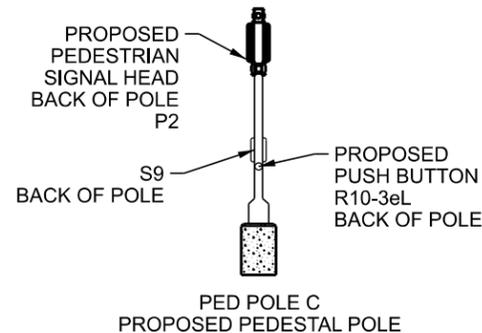


WESTBOUND ON DUVAL ROAD
N.T.S.
PROPOSED POLE, SIGNALS AND SIGNS
POLE F



SOUTHBOUND ON WEST COW PATH
N.T.S.
PROPOSED POLE, SIGNALS AND SIGNS
POLE A

NOTES:
1. PEDESTAL POLE ELEVATIONS ARE SHOWN FROM A VIEW OF LOOKING EASTBOUND ON DUVAL ROAD.



NOTES:
1. HEADS WILL BE CENTERED OVER THE LANES OR AS DIRECTED BY ENGINEER. DISTANCE SHOWN ALONG MAST ARMS ARE APPROXIMATE AND WILL BE ADJUSTED IN THE FIELD AS NEEDED.
2. FOUNDATIONS WILL BE ADJUSTED IN THE FIELD IN ORDER TO MEET CLEARANCE.
3. LOCATION OF MAST ARMS IS APPROXIMATE. ANY CHANGES WILL BE APPROVED BY ENGINEER.
4. MAST ARM ATTACHMENT HEIGHT WILL BE CALCULATED BY THE CONTRACTOR IN THE FIELD AND APPROVED BY THE ENGINEER.
5. DAMPENING PLATES TO BE INSTALLED ON THE ARMS 40' OR LONGER.

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REV. NO.	DATE	REVISION DESCRIPTION

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
3/6/2024

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
DUVAL AT W COW PATH
PROPOSED SIGNAL ELEVATIONS

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REVIEWED BY	SS	

CONDUIT AND CONDUCTOR SCHEDULE	1	2	3	4	5	6	7	8	9	10	11	12	13	14	TOTAL QTY (LF)
RUN NUMBER	73	9	10	83	13	13	67	14	52	11	312	82	110	10	
TYPE- TRENCH(T), BORE(B),EXISTING(EX)	B	T	T	B	T	T	B	T	T	T	B	B	EX	EX	
2 INCH (PVC)															0
3 INCH (PVC)	3	3	3	3				3	3	3	3	3	3	3	2457
4 INCH (PVC)					1	1		1							40
POWER (INSULATED WIRES)															0
#6 BLACK ELEC CONDR															0
#6 WHITE ELEC CONDR															0
LUMINAIRE WIRE SIZE AND TYPE															305
#10/2C	1	1	1	1				1	1	1					326
#12/2C (APS)			1	1	1	1	3	1	4		4	4	4	4	2598
SIGNAL WIRE SIZE AND TYPE															326
#14/5C (PEDESTRIAN HEADS)			1	1	1	1	1	1	2	2	1	3	3	3	2049
#14/20C (MAIN SIGNAL CABLE)	1	1	1	2											737
CCTV CAMERA CABLE															737
CAT 5E CABLE & POWER															2049
RADAR	1	1	1	2											2049
RADAR CABLE & POWER															0
GROUND															0
#6 BARE (GROUNDING FOR POWER)					1	1	3	1	3	3	3	3	3	3	2497
#6 BARE (GROUNDING FOR ALL OTHER)	3	3	3	3											

APS MESSAGE TABLE

APS UNIT #	APS MESSAGE INFORMATION		
	ACKNOWLEDGEMENT DEFAULT "WAIT"	EXTENDED PRESS MESSAGE "Wait to cross (Street Name) at (Street Name)"	WALK PRESS MESSAGE (Street Name) Walk sign is on to cross, (Street Name) or tone
P1	YES	DUVAL RD at W COW PATH	DUVAL RD
P2	YES	DUVAL RD at W COW PATH	DUVAL RD
P3	YES	W COW PATH at DUVAL RD	W COW PATH
P4	YES	W COW PATH at DUVAL RD	W COW PATH

POLE WIRING

POLE NAME	A	B	C	D	E	F	TOTAL
#12/2C		10	10	10	10		40
#14/5C		15	15	15	15		60
#14/7C	80	75					155
CCTV CABLE		35					35
LUMINAIRE CABLE	50	50				50	150
RADAR CABLE	40	45				40	125

CABLE TERMINATION CHART

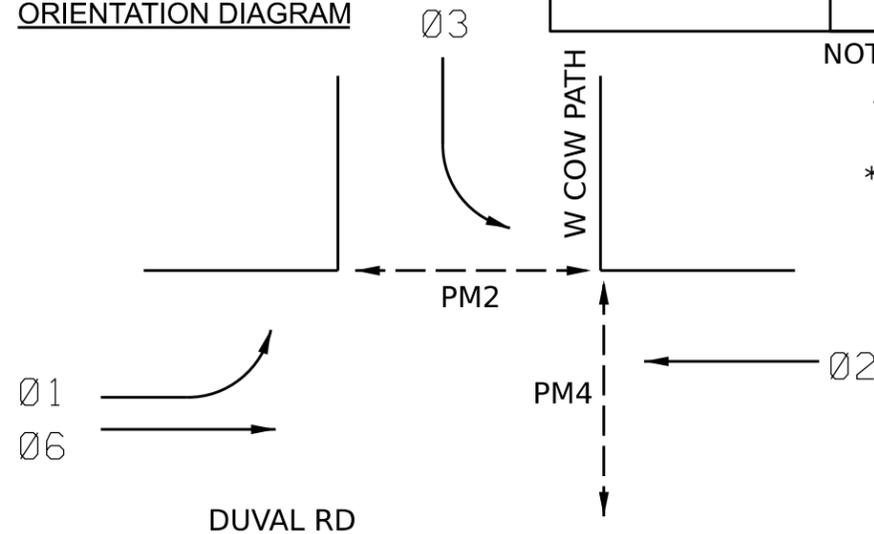
Terminal #	Conductor Color	Signal Direction	Signal Colors	CORNER 1- POLE A				CORNER 2- POLE B				CORNER 3- POLE F			
				Head 1 (LEFT) 7C	Head 2 (LEFT) 7C	Head 3 (LEFT) 7C	Head 4 (THRU) 5C	Head 5 (THRU) 5C	Ped P1 N/S 5C	Ped P2 N/S 5C	Head 6 (THRU) 5C	Head 7 (THRU) 5C	Ped P3 E/W 5C	Ped P4 E/W 5C	
1	Red/White	Thru	Red				Red	Red				Red	Red		
2	Blue/White		Yellow				Black	Black				Black	Black		
3	Green/White		Green				Green	Green				Green	Green		
4	White	Neutral					White	White				White	White		
5	Red		<- R	Red	Red	Red									
6	Orange		<- Y	Orange	Orange	Orange									
7	Black	Left Turns FYA	FYA	Black	Black	Black									
8	Blue		Spare												
9	Green		<- G	Green	Green	Green									
10	NEUTRAL JUMPER			White	White	White									
11	Red/Black	N/S Peds	Don't Walk						Red	Red					
12	Green/Black		Walk						Green	Green					
13	Black/White	E/W Peds	Don't Walk									Red	Red		
14	Blue/Black		Walk									Green	Green		
15	NEUTRAL JUMPER								White	White					
16	Red/Green	Right Turn/ Nearside	Red												
17	Orange/Red		Yellow												
18	Blue/Red		Green												
19	NEUTRAL JUMPER														
20	White/Red		R ->												
21	Orange/Black	Spare / Right Turn FYA	Y ->												
22	White/Black		FYA												
23	Black/Red		G ->												
24	NEUTRAL JUMPER														

NOTE: CONTRACTOR SHALL LAND ALL CONDUCTORS FROM THE 20C CABLE TO THE LEFT SIDE OF TERMINAL STRIP

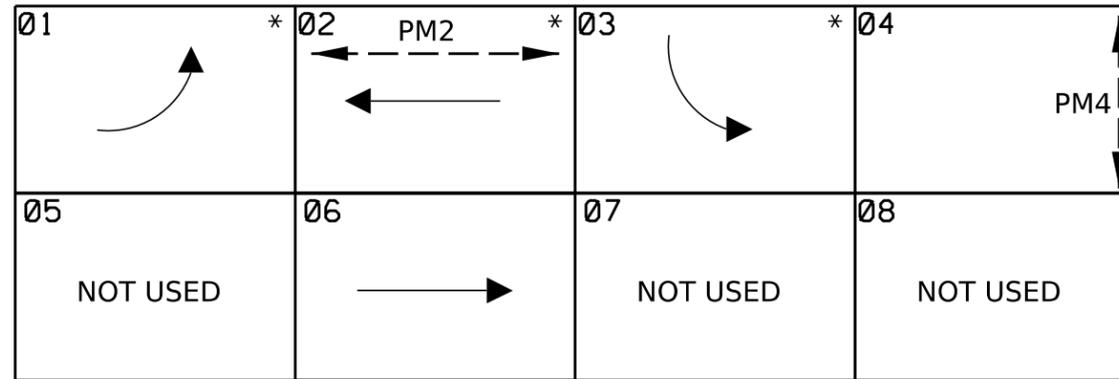
NOTES:

- RADAR CABLE IS SUBSIDIARY TO BID ITEM SS1040-T.
- CAT 5E CABLE IS SUBSIDIARY TO BID ITEM SS1044-CCTV.
- REFERENCE STANDARD SPECIFICATION 844S FOR VARIOUS TRENCHING CLASSIFICATIONS OF TRAFFIC SIGNAL CONDUIT.
- SEE CONDUIT LAYOUT SHEET FOR LOCATIONS OF CONDUIT RUN 12 THRU 14.

SUGGESTED PHASING **
ORIENTATION DIAGRAM



SUGGESTED PHASING DIAGRAM **



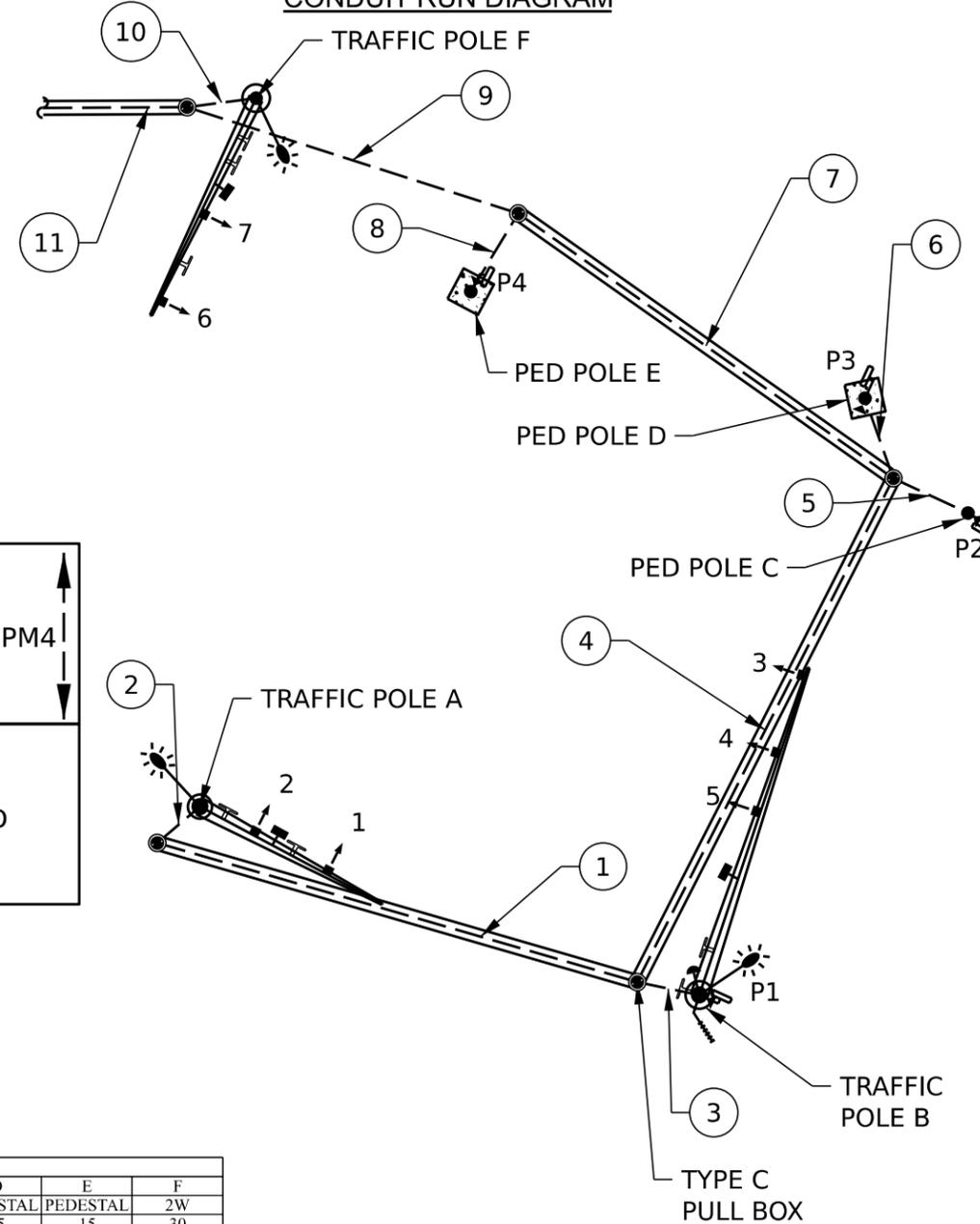
NOTE:

- * PHASES THAT CAN RUN WITH PHASE 6 CONCURRENTLY.
- ** PHASING WILL BE PROGRAMMED BY THE CITY OF AUSTIN TO COINCIDE WITH EXISTING PHASING AT THE DUVAL RD AND US 183 SIGNAL.

POLE AND FOUNDATION SCHEDULE

POLE AND FOUNDATION SCHEDULE						
POLE NAME	A	B	C	D	E	F
POLE TYPE	1W	3W	PEDESTAL	PEDESTAL	PEDESTAL	2W
POLE HEIGHT (FT)	30	30	15	15	15	30
MAST ARM LENGTH (FT)	30	50	NONE	NONE	NONE	35
LUMINAIRE ARM	YES	YES	NONE	NONE	NONE	YES
LUMINAIRE (250 EW EQ LED)	YES	YES	NONE	NONE	NONE	YES
FOUNDATION DIAMETER (IN)	42	48	24	60	60	48
FOUNDATION DEPTH (FT)	16	18	4	1.5	1.5	17
BOLT CIRCLE DIA. (IN)	20	21	NONE	NONE	NONE	20
ANCHOR BOLT DIA. (IN)	1.75	2	NONE	NONE	NONE	1.75

CONDUIT RUN DIAGRAM



File No. 25-1303

REVISION DESCRIPTION

DATE

REVIEW BY

DATE

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
3/6/2024
3/6/2024

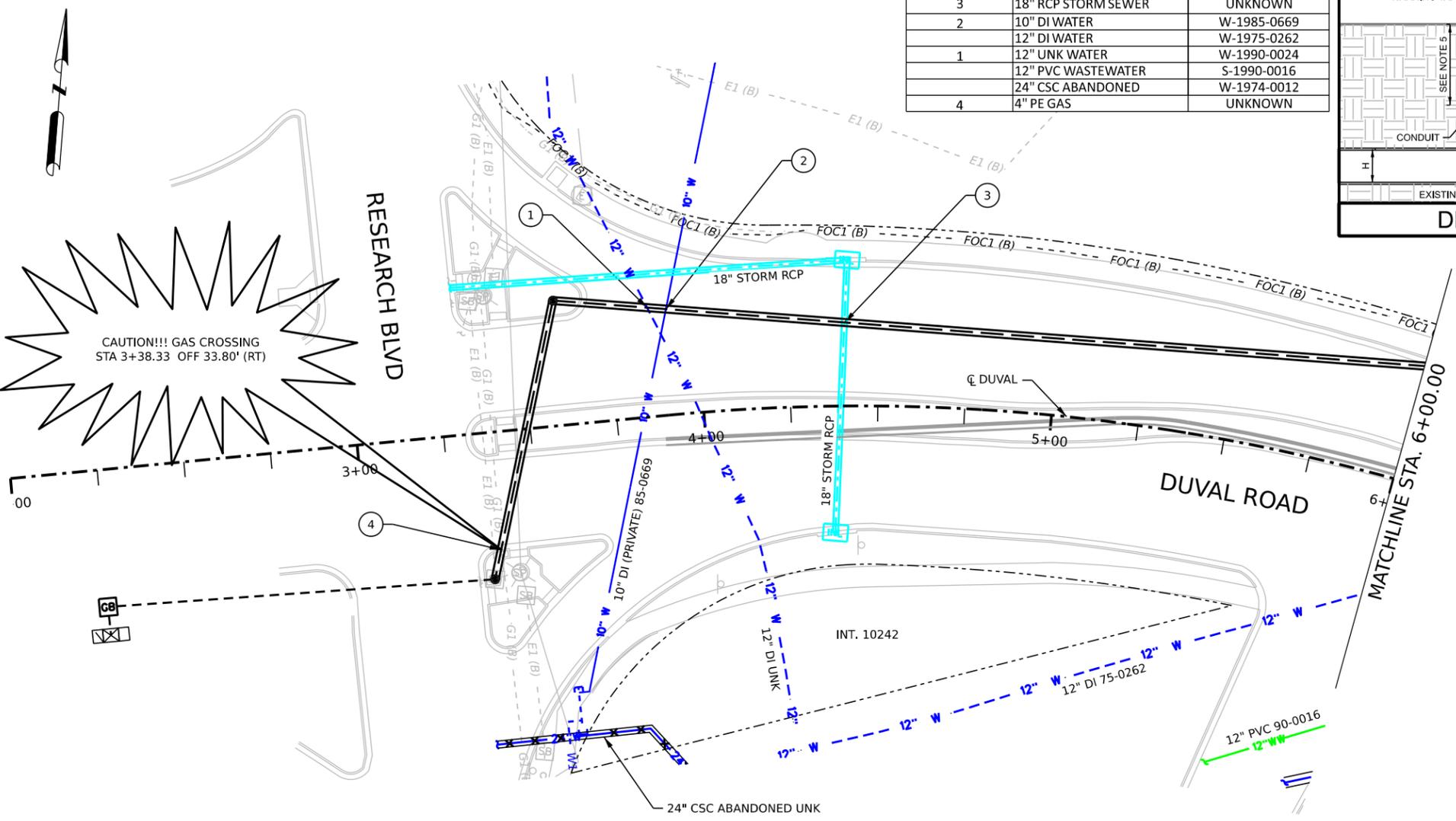
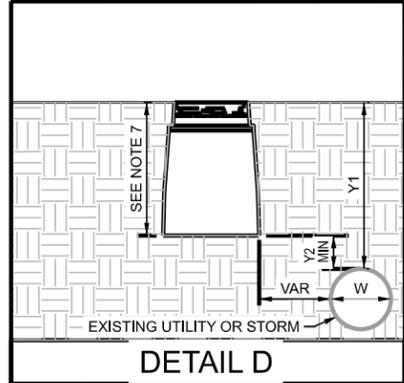
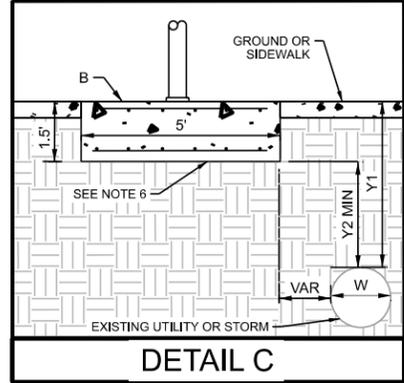
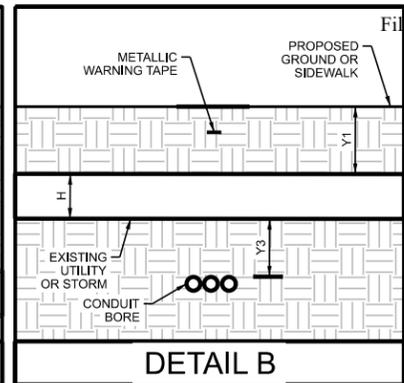
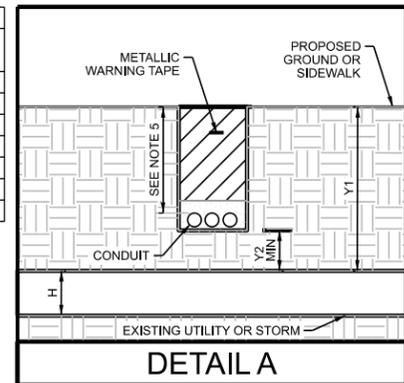
CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
DUVAL AT W COW PATH
CONDUIT AND CONDUCTOR SCHEDULE

BGE Inc.
1701 Directors Boulevard, Suite 1000
Austin, TX 78744
Tel: 512-479-4400 • www.bgeinc.com
TBPE Registration No. F-1046

NOTES	NAME	DATE
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DRAWN BY	HK	
DESIGNED BY	HK	
CHECKED BY	MB	
REVIEWED BY	SS	

45 OF 61

AUSTIN UTILITY PROJECT #		
CROSSING LOCATION(S)	UTILITY	PROJECT #
3	18" RCP STORM SEWER	UNKNOWN
2	10" DI WATER	W-1985-0669
1	12" DI WATER	W-1975-0262
	12" UNK WATER	W-1990-0024
	12" PVC WASTEWATER	S-1990-0016
	24" CSC ABANDONED	W-1974-0012
4	4" PE GAS	UNKNOWN



- NOTES:
- DETAILS ARE FOR PEDESTAL POLES, SIGNAL POLES, GROUND BOXES, AND CONDUITS CROSSING OR IN THE VICINITY OF EXISTING UTILITIES. CONTRACTOR TO COORDINATE WITH ALL UTILITY COMPANIES TO FIELD-VERIFY UTILITY LOCATIONS PRIOR TO BEGINNING CONSTRUCTION.
 - THE DEPTHS OF THE UTILITY LINES ARE BASED ON RECORD AS-BUILT DRAWINGS. THESE ARE NOT BASED ON LEVEL A SUE OR POTHOLES. AS-BUILTS MAY CONTAIN INCOMPLETE OR INCORRECT DATA DUE TO THE AGE OF INSTALLATION.
 - WHERE THERE IS NO AS-BUILT INFORMATION AVAILABLE (N/A), MAINTAIN A 2-FT VERTICAL CLEARANCE BETWEEN PROPOSED SIGNAL EQUIPMENT AND EXISTING UTILITIES. CONTRACTOR TO FIELD LOCATE, PROTECT, AND USE CAUTION TO AVOID UTILITY.
 - CONTACT ONE CALL FOR LOCATES; MAINTAIN A 2-FT VERTICAL AND A 5-FT HORIZONTAL CLEARANCE FROM ALL DISTRIBUTION GAS LINES. USE CAUTION WHEN DIGGING NEAR TGS LINES. ENTITY THAT CAUSES DAMAGES TO TGS LINES IS RESPONSIBLE FOR ALL COSTS. DIAL 1-800-DIG-TESS AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION TO HAVE GAS LINES LOCATED. WHEN DIGGING WITHIN 10' OF A TRANSMISSION OR HPD LINE, PLEASE CONTACT EDDIE MARENCO (P: 512-423-8466; EMAIL: EDWARD.MARENCO@ONEGAS.COM) AT LEAST 48 HOURS IN ADVANCE SO THAT A TGS REPRESENTATIVE CAN BE SCHEDULED TO BE PRESENT DURING ALL EXCAVATION ACTIVITIES.
 - REFER TO CONDUIT AND CONDUCTOR SCHEDULE FOR CONDUIT RUN INFORMATION. SEE COA STANDARD DETAIL 835S-1 FOR TRENCH CONDUIT INSTALLATION.
 - REFER TO THE POLE AND FOUNDATION SCHEDULE FOR PEDESTAL AND SIGNAL POLE INFORMATION. SEE PEDESTAL POLE SPREAD FOOTING DETAILS FOR FOUNDATION DIMENSIONS. SEE COA STANDARD 831S-2 DETAILS FOR POLE DIMENSIONS.
 - SEE COA STANDARD DETAILS 834S-3 AND 834S-4 FOR TYPE B GROUND BOX DIMENSIONS. SEE COA STANDARD DETAILS 834S-5 AND 834S-6 FOR TYPE C GROUND BOX DIMENSIONS.
 - H (FT) = HEIGHT OF UTILITY, IN FEET.
 - W (FT) = WIDTH OF UTILITY, IN FEET.
 - Y1 (FT) = DISTANCE BETWEEN GROUND SURFACE AND TOP OF UTILITY, IN FEET.
 - Y2 MIN (FT) = MINIMUM DISTANCE BETWEEN BOTTOM OF PROPOSED EQUIPMENT AND TOP OF UTILITY, IN FEET.
 - Y3 (FT) = DISTANCE BETWEEN BOTTOM OF UTILITY AND TOP OF PROPOSED EQUIPMENT, IN FEET.

CALLOUT #	UTILITY CROSSING	PROPOSED EQUIPMENT AT UTILITY CROSSING	LOCATION OF CROSSING			DIMENSIONS BETWEEN EQUIPMENT AND CROSSING					**SOFFIT ELEVATION OF UTILITY CROSSING	GROUND ELEVATION FROM SURVEY	
			ALIGNMENT	STATION	OFFSET	DETAIL UTILIZED	H (FT)	W (FT)	Y1 (FT)	Y2 MIN (FT)			Y3 (FT)
1	WATER 12" UNK	CONDUIT RUN #12	CL_DUVAL	3+86.68	31.27' LT	B	1.00	1.00	9.31	-	5.00	842.50	851.73
2	WATER 10" DI	CONDUIT RUN #12	CL_DUVAL	3+91.89	30.36' LT	B	0.83	0.83	N/A	-	5.00	N/A	851.63
3	STORM 18" RCP	CONDUIT RUN #12	CL_DUVAL	4+40.62	23.95' LT	B	1.50	1.50	N/A	-	5.00	N/A	850.78
4	GAS 4" PE	CONDUIT RUN #16	CL_DUVAL	3+38.33	33.80' RT	B	0.33	0.33	N/A	-	5.00	N/A	852.85

GENERAL NOTE: DATA GATHERED FROM AS-BUILTS. ALL DATA IS ASSUMED PER UCM. DISPLAYED DEPTHS ARE MEASURED FROM EXISTING GROUND TO INSIDE SOFFIT OF UTILITY PIPE UNLESS OTHERWISE NOTED.
** = UTILITY ELEVATION INFORMATION OBTAINED FROM AVAILABLE PROJECT RECORD SOURCES FOUND ON THE AUSTIN WATER RECORDS ACCESS APPLICATION ARC GIS SERVER.

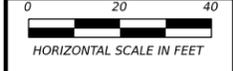
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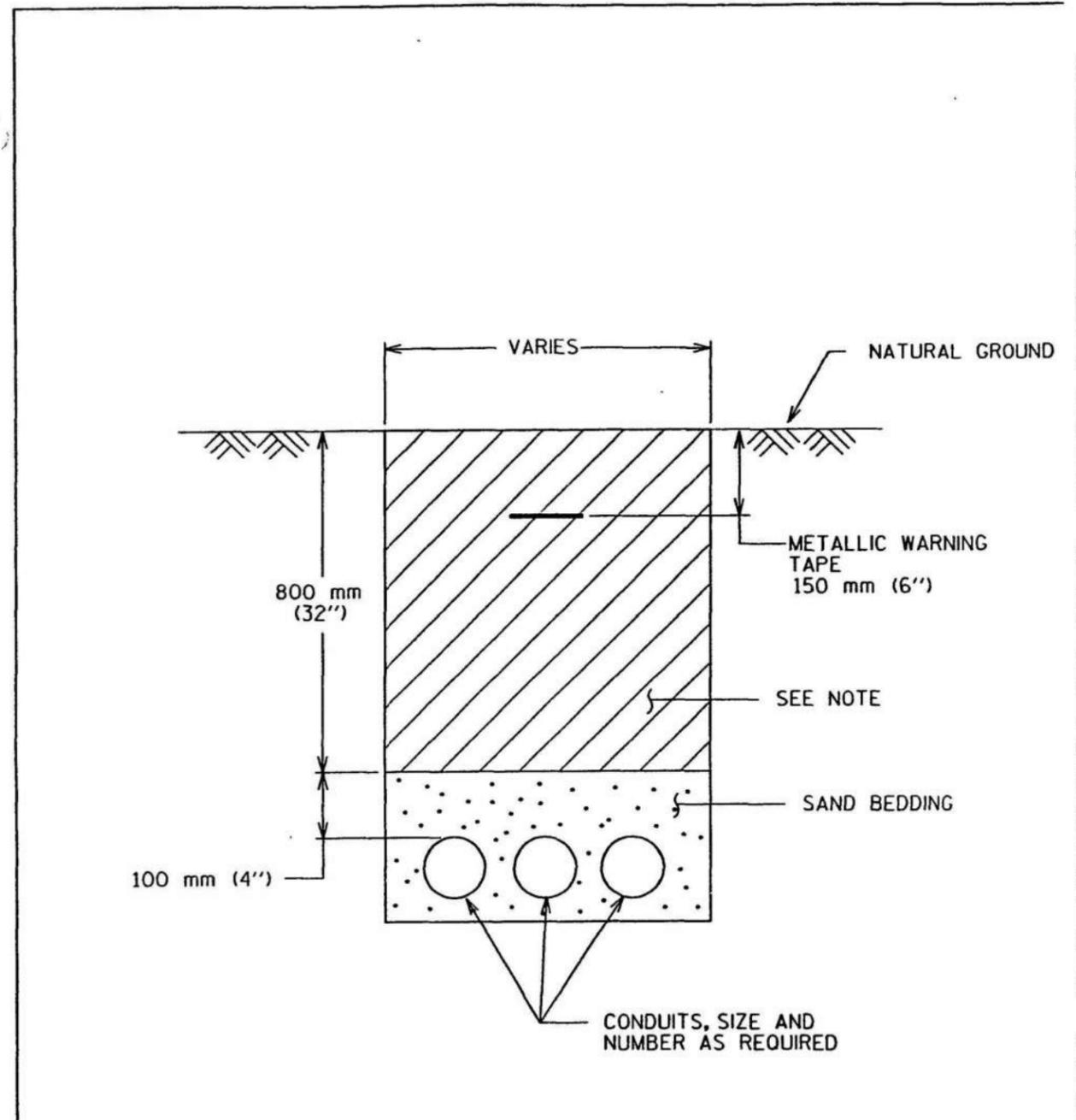
CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
AUSTIN WATER CLEARANCE DETAILS
DUVAL AT W COW PATH
STATION 2+00.00 TO 6+00.00



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REVIEWED BY		



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NOTE:
BACKFILL FOR TRENCHING BEHIND THE CURB (OR EDGE OF SHOULDER WHERE NO CURB EXISTS) SHALL BE COMPACTED TO THE DENSITY OF THE SURROUNDING SOIL. BACKFILL FOR OPEN CUT TRENCHING IN STREETS, DRIVEWAYS, SIDEWALKS OR MEDIANS SHALL CONFORM TO ALL REQUIREMENTS CONTAINED IN THE CITY OF AUSTIN'S UTILITY CRITERIA MANUAL, SECTION 5.

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS	TRENCH DETAIL FOR TRAFFIC SIGNAL CONDUIT
<i>Leon Baula, P.E.</i> 9/17/01 ADOPTED	STANDARD NO. 835S-1
THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	

REV. NO.	DATE	REVISION DESCRIPTION

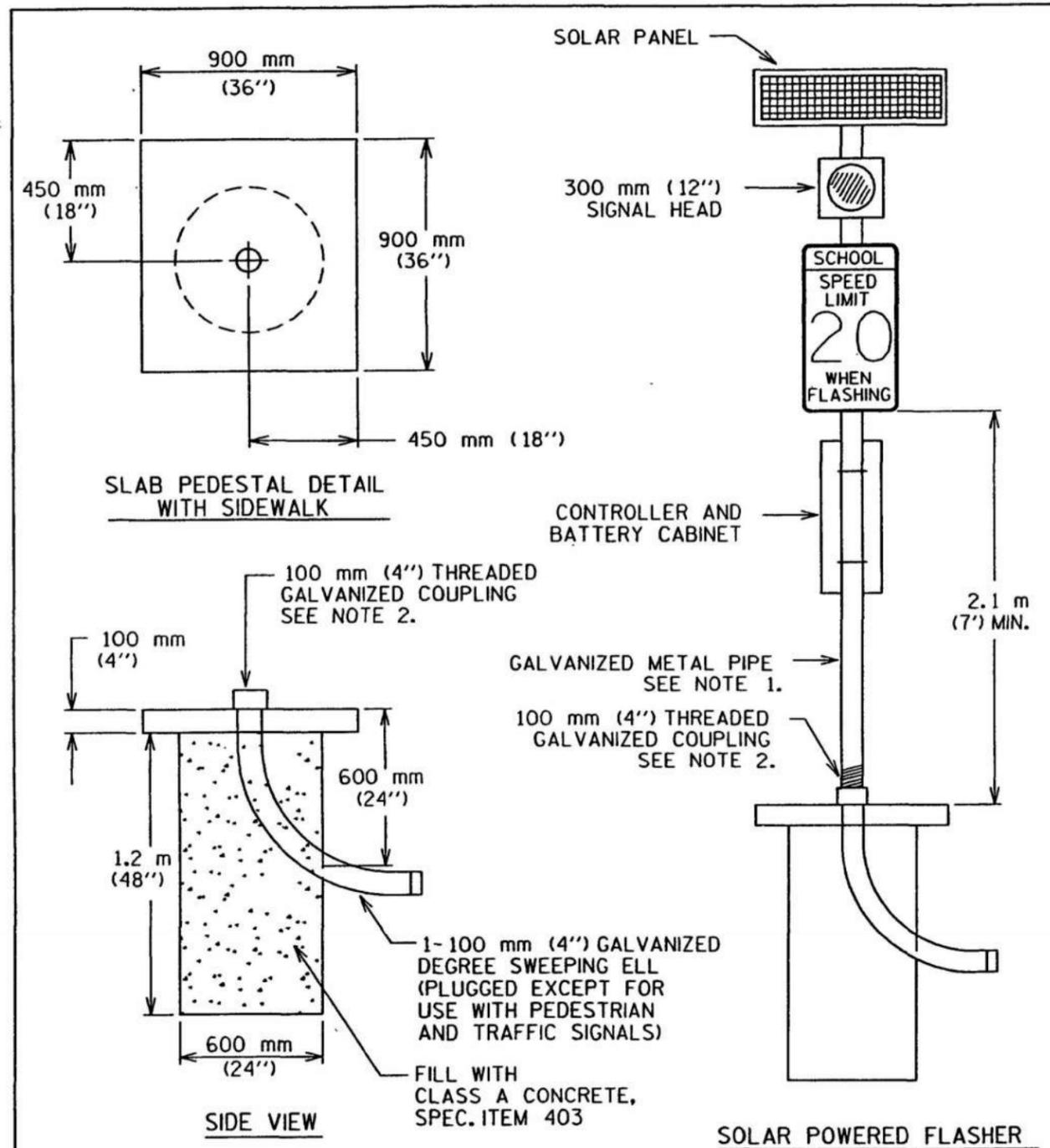
MATTHEW E. BAKER
 140924
 LICENSED PROFESSIONAL ENGINEER
Matthew Baker
 3/6/2024

3/6/2024
 CITY OF AUSTIN, TEXAS
 AUSTIN TRANSPORTATION
 DUVAL AT W COW PATH
 INTERSECTION IMPROVEMENTS
 AUSTIN STANDARD
 TRENCH DETAIL FOR SIGNAL CONDUIT

BGE
 BGE, Inc.
 1701 Directors Boulevard, Suite 1000
 Austin, TX 78744
 Tel: 512-479-4400 • www.bgeinc.com
 TBPE Registration No. F-1046

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NOTES:

1. 4.5 m X 100 mm (15' X 4") RIGID INTERMEDIATE GALVANIZED METAL PIPE 106 mm (4 1/4") NOMINAL I.D. WITH 3 mm (1/8") WALL THICKNESS, 13 KG (6 LBS) PER FOOT.
2. ENSURE COUPLING IS LEVEL AND PLUMB AND EXTENDS ABOVE THE SURFACE.

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS	SOLAR POWERED FLASHER ASSEMBLY
<i>Sean Burke, P.E.</i> 7/17/01 ADOPTED	STANDARD NO. 831S-2
THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	

REV. NO.	DATE	REVISION DESCRIPTION

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
Matthew Baker
3/6/2024

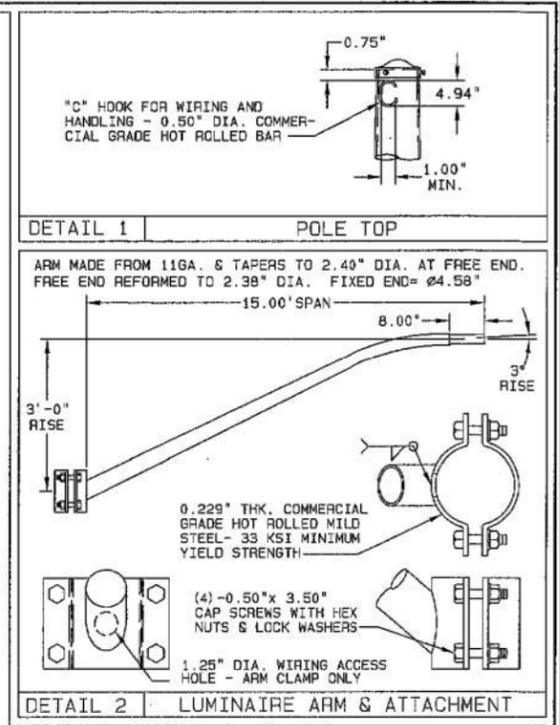
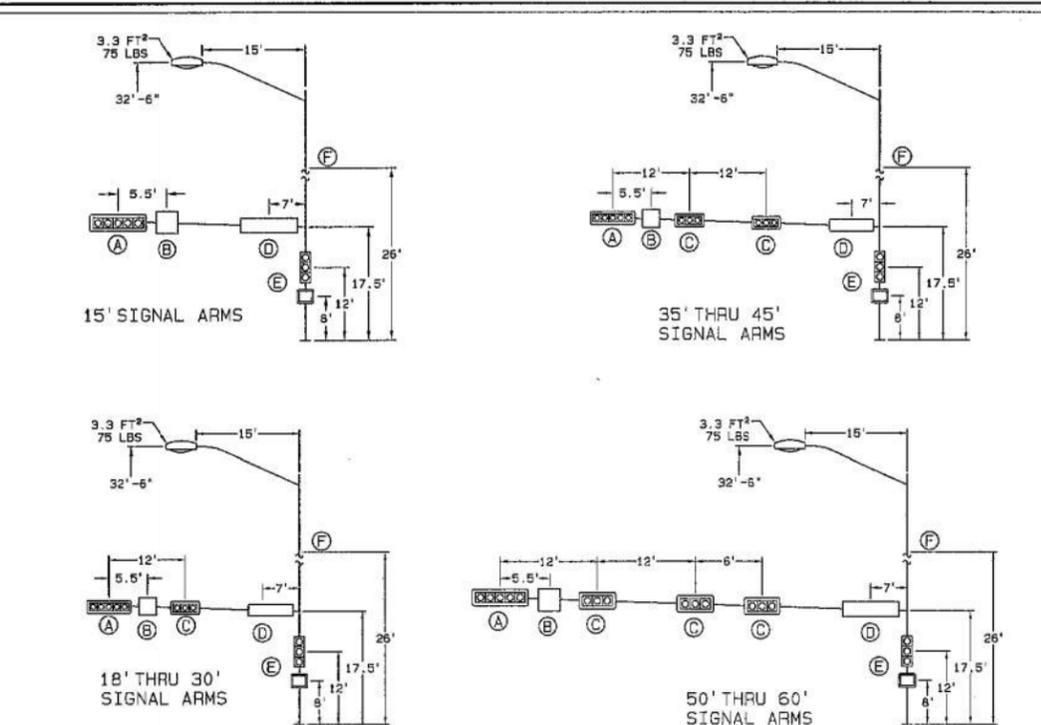
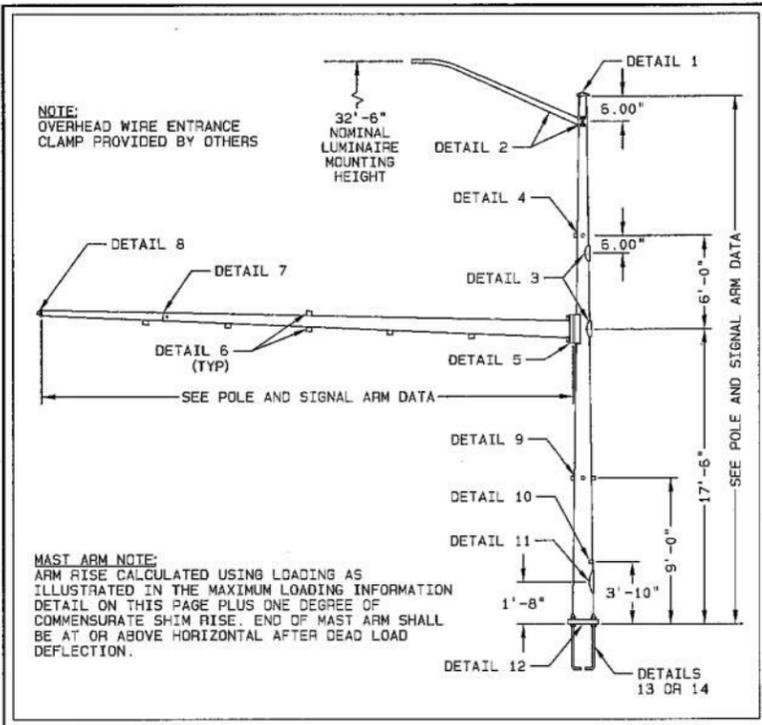
CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
AUSTIN STANDARD: SOLAR POWERED
FLASHER ASSEMBLY

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REV. NO.	DATE	REVISION DESCRIPTION



MATERIAL DATA

COMPONENT	ASTM DESIGNATION	MIN. YIELD (KSI)	COMPONENT	ASTM DESIGNATION	MIN. YIELD (KSI)
POLE SHAFTS (TYPE 1)	A595 GR. A	55	LUM. ARM CONN. BOLTS	A325	
POLE SHAFTS (TYPE 2&3)	A572 GR. 65	65	ANCHOR BOLTS	F1554 GR. 55	55
MAST ARM SHAFTS	A595 GR. A	55	GALVANIZING	A123	
LUMINAIRE ARM SHAFT	A595 GR. A	55	GALVANIZING-HARDWARE	HOT DIP ZINC	
BASE PLATES	A36	36			
ARM ATTACHMENTS	A36	36			
MAST ARM CONN. BOLTS	A325				

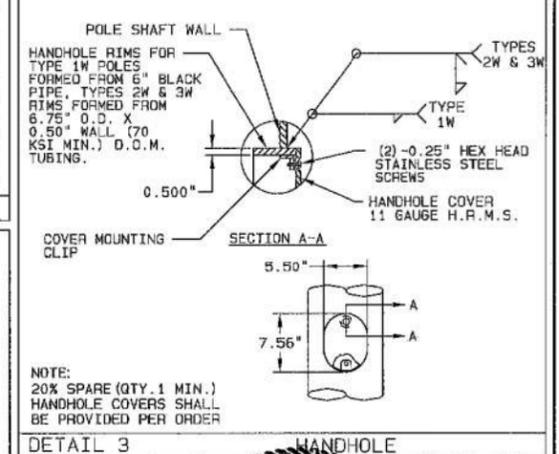
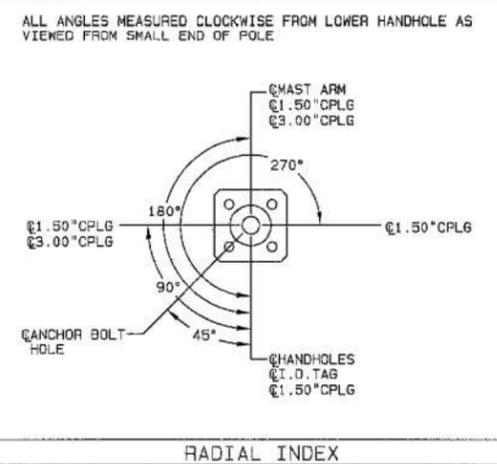
DESIGN CRITERIA:
1994 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS.
WIND VELOCITY:
80 MPH ISOTACH.

MAX. LOADING INFORMATION

DEVICE	DESCRIPTION	PROJ. AREA (SQ. FT.)	WEIGHT (LBS.)
(A)	12"-5 SEC. SIGNAL W/BACKPLATES	15.00	85
(B)	36" X 36" DIRECTIONAL SIGN	9.00	27
(C)	12"-3 SEC. SIGNAL W/BACKPLATES	9.00	53
(D)	26" X 84" STREET NAME SIGN	15.00	36
(E)	PEDESTRIAN SIGNAL HEADS	8.00	80
(F)	OVERHEAD WIRE ENTRANCE	64.00	200

POLE AND SIGNAL ARM DATA

POLE TYPE	POLE TUBE				SIGNAL ARM TUBE				SIGNAL ARM COUPLING LOCATIONS & ORIENTATIONS (*OPTIONAL SIGNAL SPACING)					
	BASE DIA. (IN)	TOP DIA. (IN)	LENGTH (FT)	GAUGE OR THK. (IN)	ARM SPAN (FT)	FIXED END DIA. (IN)	FREE END DIA. (IN)	GAUGE OR THICK (IN)	COUPLING DISTANCE FROM END OF ARM. / ANGLES CW. FROM TOP OF SIMPLEX AS VIEWED FROM SMALL END OF MAST ARM.					
1W	12.50	8.30	30.00	3	15.00	6.00	3.90	7						7.5' / 0°
					18.00	6.00	3.48	7	12' / 180°					9' / 0°
					20.00	6.00	3.20	7	12' / 180°					10' / 0°
					25.00	7.00	3.50	7	12' / 180°					12.5' / 0°
					30.00	8.00	3.80	7	12' / 180°					15' / 0°
2W	13.50	9.30	30.00	0.2188	35.00	9.00	4.10	7	12' / 180°	24' / 180°				17.5' / 0°
					40.00	10.00	4.40	7	12' / 180°	*18' / 180°	24' / 180°	*30' / 180°		20' / 0°
					45.00	10.50	4.20	7	12' / 180°	*18' / 180°	24' / 180°	*30' / 180°		22.5' / 0°
3W	16.00	11.80	30.00	0.2188	50.00	12.00	5.00	7	12' / 180°	*18' / 180°	24' / 180°	*30' / 180°	36' / 180°	25' / 0°
					55.00	13.00	5.53	DET. 7	12' / 180°	*18' / 180°	24' / 180°	*30' / 180°	36' / 180°	27.5' / 0°
					60.00	13.00	4.83	DET. 7	12' / 180°	*18' / 180°	24' / 180°	*30' / 180°	36' / 180°	30' / 0°



STATE OF TEXAS
BARRY N. SLADEK
91283
LICENSED PROFESSIONAL ENGINEER
7/31/13

REV	DRAWN BY-DATE	CHECK BY-DATE	DESCRIPTION
0	AC19 07/30/13	AC19 07/30/13	LOC. BASE HH/TOP HH'S TO BE THE SAME
1	DJT 05/06/01		

TITLE CITY OF AUSTIN, TX.
MASTARM TRAFFIC STRUCTURES
Special Detail #12

VALMONT INDUSTRIES, INC. RESERVES THE RIGHT TO INSTALL VARIOUS, ENGINEER APPROVED, MATERIAL HANGING ACCOMMODATIONS TO FACILITATE THE MANUFACTURING PROCESS.

valmont
Valley, NE 68064
(402) 359-2201

PAGE NUMBER: 1 OF 2
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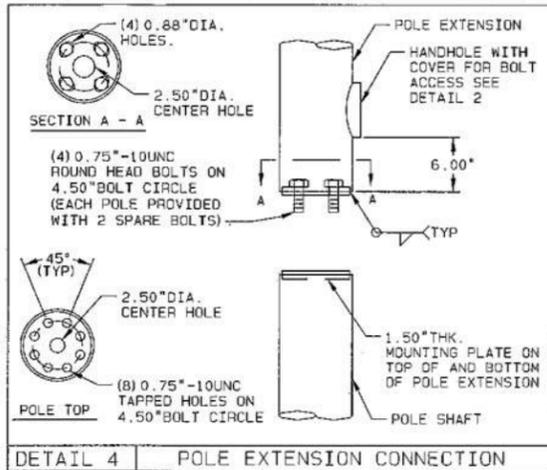
STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
3/6/2024

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
AUSTIN STANDARD
MAST ARM TRAFFIC STRUCTURES (1 OF 2)

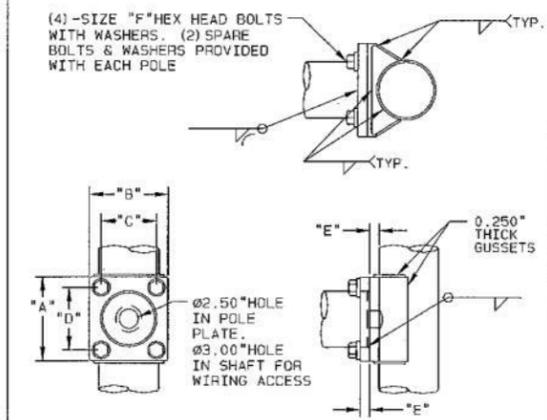
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NOTES	NAME	DATE

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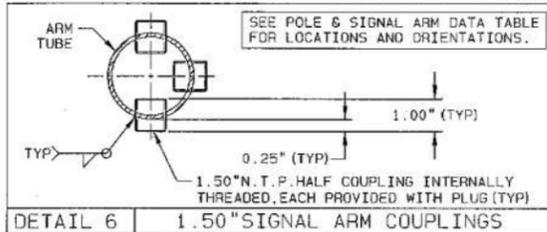


DETAIL 4 POLE EXTENSION CONNECTION

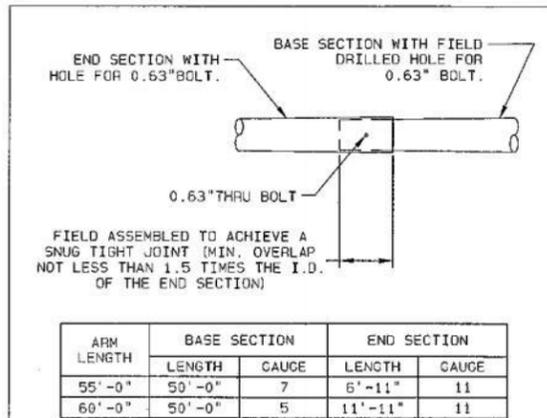


DETAIL 5 SIGNAL ARM ATTACHMENT

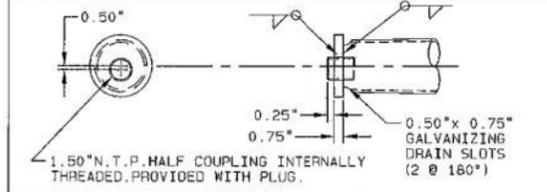
SIGNAL ARM ATTACHMENT DATA						
POLE TYPE	"A" (IN)	"B" (IN)	"C" (IN)	"D" (IN)	"E" (IN)	"F" (IN)
1	18.00	9.75	6.50	14.00	2.00	1.25"-7UNC X 4.25"LG
2	18.00	18.00	14.75	14.75	2.00	1.25"-7UNC X 4.25"LG
3	19.25	19.25	16.00	16.00	2.00	1.25"-7UNC X 4.25"LG



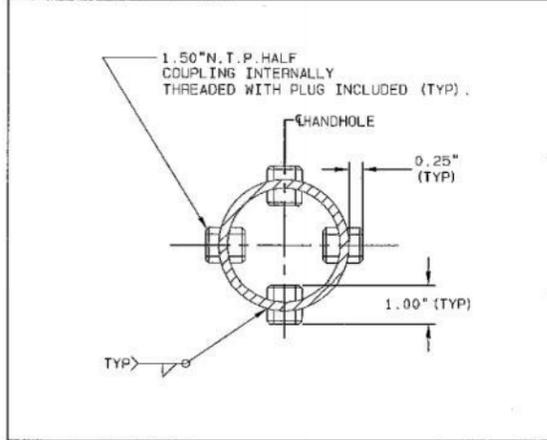
DETAIL 6 1.50" SIGNAL ARM COUPLINGS



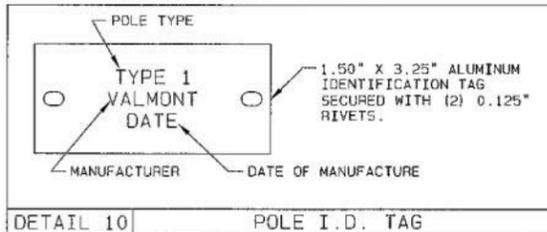
DETAIL 7 SIGNAL ARM SLIP JOINT



DETAIL 8 SIGNAL ARM END BRACKET

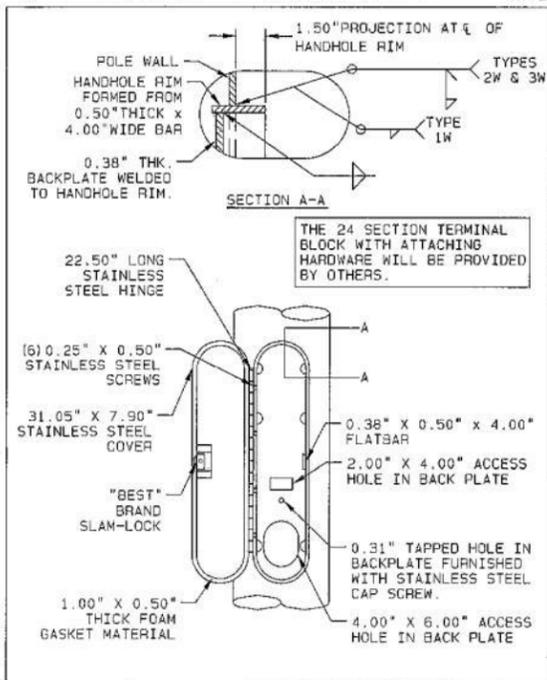


DETAIL 9 1.50" POLE COUPLINGS

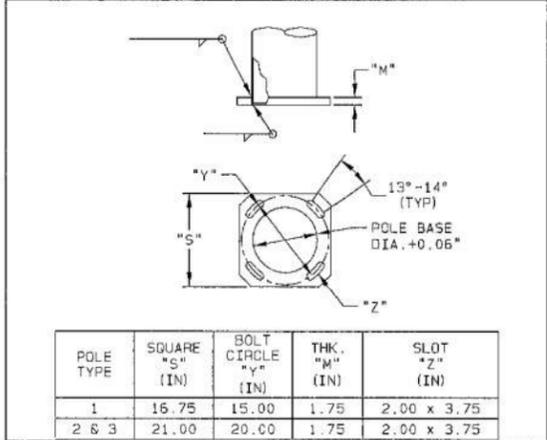


DETAIL 10 POLE I.D. TAG

TITLE CITY OF AUSTIN, TX.
MASTARM & DAVIT EXTENSION TRAFFIC STRUCTURES

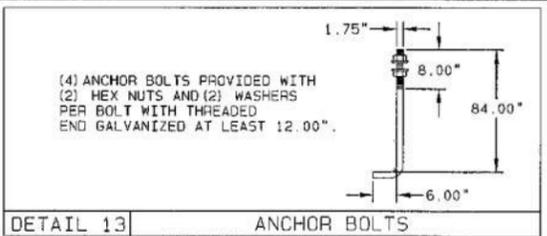


DETAIL 11 BASE HANDHOLE



DETAIL 12 POLE BASE

POLE TYPE	SQUARE "S" (IN)	BOLT CIRCLE "Y" (IN)	THK. "M" (IN)	SLOT "Z" (IN)
1	15.75	15.00	1.75	2.00 x 3.75
2 & 3	21.00	20.00	1.75	2.00 x 3.75



DETAIL 13 ANCHOR BOLTS

REV. NO.	DATE	REVISION DESCRIPTION

STATE OF TEXAS
MATTHEW E. BAKER
140924
LICENSED PROFESSIONAL ENGINEER
3/6/2024

CITY OF AUSTIN, TEXAS
AUSTIN TRANSPORTATION
DUVAL AT W COW PATH
INTERSECTION IMPROVEMENTS
AUSTIN STANDARD: MAST ARM &
DAVIT EXTENSION TRAFFIC STRUCTURES (2 OF 2)

STATE OF TEXAS
BARRY N. SLADEK
91283
LICENSED PROFESSIONAL ENGINEER
Barr N Sladek
8/8/13

BGE
BGE, Inc.
1701 Directors Boulevard, Suite 1000
Austin, TX 78744
Tel: 512-479-4400 • www.bgeinc.com
TBPE Registration No. F-1046

NOTES	NAME	DATE

DATE: 3/6/2024 8:09:49 AM
FILE: G:\TXC\Projects\BOE Consulting\10941-00 Duval at W Cow Path\03 CADD\01 Shts\13-SGNL\SIGAS\DUVAL SIGNAL DETAILS_DB00591_4.dgn

valmont
Valley, NE 68064
(402) 359-2201

PAGE NUMBER: 2 OF 3
DRAWING NUMBER: DB00410
REV: B

From: [Carlos Madriz](#)
To: [Sirwaitis, Sherri](#)
Cc: [C M](#)
Subject: Case C144-2024-0170 - Concerns of a property owner of neighboring Angus Valley community.
Date: Thursday, April 17, 2025 1:13:33 PM

Y

External Email - Exercise Caution

Dear Ms. Sirwaitis,

I have followed up closely on the request for rezoning mentioned above to develop approximately 1,200 multi-family units and retail/restaurant uses instead of the commercial office park approved on the former 3 M lot.

As a property owner in the Angus Valley neighborhood and a city taxpayer, I would like to let you know of my high-level concerns about the impact of the imminent approval for rezoning the property from commercial to multi-family use:

- Safety driving: in and out of the neighborhood at the already chaotic Duval/West Cow Path intersection. Safe turns in and out of the 3 M lot.
- Environmental concerns: additional lightning, noise, and other impacts on the surroundings.
- Safety walking: pedestrian safety despite vehicular circulation all around the external perimeter of the 3 M lot, more specifically at the West Cow Path side.
- The look and feel of green areas: fencing and green areas in the section of the lot facing Duval and West Cow Path streets.
- General Safety and Security in our neighborhood: It is a known fact that adding people impacts safety and security in the area.

As you can assess, the above are not uncommon or unfixable concerns for a project of the scope being proposed by the developer.

I'd like people reviewing this rezoning project's feasibility to consider those concerns, inform the developer, and request fixes or mitigations.

If you need more information or clarification, please do not hesitate to contact me.

Awaiting your reply, I remain.

Sincerely,

Carlos Madriz

4905 Pony Chase
Austin, TX

--

Carlos L. Madriz

LinkedIn: <http://www.linkedin.com/in/carlosmadriz>

CAUTION: This is an EXTERNAL email. Please use caution when clicking links or opening attachments. If you believe this to be a malicious or phishing email, please report it using the "Report Message" button in Outlook.
For any additional questions or concerns, contact CSIRT at "cybersecurity@austintexas.gov".

Stacey Peterson
President, Angus Valley Area Neighborhood Association
12300 Audane Dr.
Austin, TX 78727
bc-Hank.Smith@austintexas.gov

May 30, 2025

Hank Smith, Chair
Zoning and Platting Commission
City of Austin

Re: Support for Rezoning Case C14-2024-0170 (Duval Rd.)

Dear Chair Smith:

I am writing on behalf of the Angus Valley Area Neighborhood Association (AVANA) to express our support for Rezoning Case C14-2024-0170 concerning the property on Duval Rd. & 183.

After some honest and productive conversations with the developer, we were able to reach an agreement that reflects real give-and-take. The signed Memorandum of Understanding outlines compromises that matter to us such as improved buffer zones, fencing, and other site considerations that will help protect the character of the surrounding neighborhood.

We are grateful that the developer was willing to listen and adjust. With that in mind, AVANA supports this rezoning request, and we hope the Commission will approve it in alignment with the terms spelled out in the MOU (attached here for reference).

Thank you for your time and for the work you do on behalf of Austin's neighborhoods.

Sincerely,
Stacey Peterson
President, AVANA

Attachment: Signed MOU for C14-2024-0170

MEMORANDUM

TO: Stacey Peterson, Angus Valley Area Neighborhood Association (“AVANA”)

CC: Richard T. Suttle, Jr., Armbrust & Brown, PLLC
Michael McGlashan, Karlin Real Estate
Joe Prochot, Karlin Real Estate
Clayton Baca, Trammell Crow Company

FROM: Karlin Duval, LLC (the “Landowner”)

DATE: May 30, 2025

RE: Memorandum of Understanding between AVANA and Karlin Duval, LLC regarding the rezoning case for the Duval PDA in review under City of Austin Case No. C14-2024-0170 (the “Rezoning Case”)

This memorandum of understanding is written to outline the terms of the outstanding commitments made between Karlin Duval, LLC and AVANA in connection with the Rezoning Case. In exchange for the support of the Angus Valley Area Neighborhood Association, the Landowner has agreed to the following commitments:

1. The conditions outlined in the letter agreement dated October 14, 2021 regarding zoning case C14-2020-0146 (the “Letter Agreement”) shall be satisfied with the exception that a restrictive covenant is not required to be filed with the Recording Division of the Clerk of Travis County, Texas. To date the 8-ft adjoining fence has been constructed and will be maintained by the Landowner, the Public Utility Easement is in place as documented on the plat for the Resubdivision of Lot 1-A, Resubdivision of a 35.207 acre portion of Lot 1, Research Park, Lot 2, and a portion of Lot 3, Research Park recorded in Document No. 202300116 of the Official Public Records of Travis County, Texas (the “Resubdivision”), and the 50-ft residential buffer has been established and maintained. The fence, easement, and 50-ft residential buffer described in the Letter Agreement shall be clearly shown on future site development plans (permits) and plat. A copy of the Letter Agreement and plat for the Resubdivision of Lot 1-A, Resubdivision of a 35.207 acre portion of Lot 1, Research Park, Lot 2, and a portion of Lot 3, Research Park are provided as **Exhibit A** and **Exhibit B**, respectively.
2. The Landowner will provide timely notice to AVANA regarding any development applications submitted to the City of Austin, including access to submitted site plan documents at the time of their filing.
3. A 15-ft residential buffer shall be established and maintained along the portion of the property line of Lot 7 of the Resubdivision that adjoins the single family residences along Bull Run Circle. The approximate location of the 15-ft residential buffer is identified in yellow in **Exhibit B**. The residential buffer shall be defined

as a 15-ft wide vegetative buffer strip where native trees, shrubbery and grasses shall be preserved. The Landowner shall maintain the buffer area in its natural state, undisturbed and free of encroachments, including but not limited to any permanent or temporary structures, roadways, pedestrian trails, or picnic areas unless otherwise required by the City of Austin during the site development permit process. Any new easements required by a utility provider may be constructed in the buffer area provided that the area disturbed for such improvements shall be promptly revegetated with native trees, shrubbery, and grasses and allowed to regrow in its natural state in accordance with applicable requirements and approvals of such utility providers.

We appreciate the opportunity to work with AVANA regarding the Rezoning Case. If AVANA is in agreement with the terms outlined above, please sign this letter where indicated below.

Sincerely,



Karin Duval, LLC
Matthew Schwab

ACCEPTED:

Angus Valley Area Neighborhood Association

By: Stacey Peterson
Name: Stacey Peterson, President
Title: Angus Valley Area Neighborhood Association